

AT A REGULAR MEETING OF THE DEVELOPMENT REVIEW COMMITTEE OF THE COUNTY OF JAMES CITY, VIRGINIA, HELD IN THE BUILDING E CONFERENCE ROOM AT 4:00 P.M. ON THE 2nd DAY OF JUNE, TWO THOUSAND FOUR.

ROLL CALL

Mr. Jack Fraley
Mr. Don Hunt
Mr. Joe McCleary
Ms. Peggy Wildman

ALSO PRESENT

Mr. David Anderson, Senior Planner
Ms. Karen Drake, Senior Planner
Mr. Chris Johnson, Senior Planner
Ms. Sarah Weisiger, Planner
Mr. Darryl Cook, Environmental Division Director
Mr. Mark Hill, Fire Department
Mr. Leo Rogers, Deputy County Attorney

MINUTES

Following a motion by Mr. Hunt, the DRC approved the minutes as corrected from the April 28th, 2004 meeting by a unanimous voice vote.

CONSENT CASES

CASE NO. C-055-04. MID-COUNTY PARK TRAIL

CASE NO. SP-057-04. ARCHAEAIRIUM AT HISTORIC JAMESTOWN

Consent items were considered. Following a motion by Mr. Hunt, the DRC recommended approval of the cases by a unanimous voice vote.

CASE NO. SP-27-04. GREENSPRINGS CONDOMINIUMS SITE PLAN AMENDMENT

Ms. Drake presented the staff report stating this project had been deferred from the April 28th DRC meeting due to outstanding Environmental concerns. Revised plans were resubmitted on May 10th that the Environmental Division had reviewed generally and now recommended preliminary approval subject to forthcoming detailed comments. Mr. Darryl Cook confirmed for Mr. Fraley that while there were numerous detailed comments previously issued, the major issues had been resolved and Environmental was comfortable recommending preliminary approval at this time. Mr. Marc Sharp, the applicant was present but did not have any comments in response to Mr. McCleary's inquiry. There being no further

discussion and following a motion by Mr. Fraley that Mr. Hunt seconded, the DRC recommended preliminary approval be issued for the site plan subject to agency comments being addressed.

CASE NO. SP-18-04. NEW TOWN BLOCK 8, PHASE 1B RESIDENTIAL

Ms. Drake presented the staff report saying this case had been deferred from the March 31st DRC meeting due to outstanding Environmental and Fire Department comments. Ms. Drake noted that while the Environmental Division was still reviewing the engineering details, the major issues had been resolved so that preliminary approval could now be issued. Mr. Mark Hill noted the applicant's difficulty in altering their design to mitigate the fire hazards detailed his May 18th memorandum; however the Fire Department wishes to identify these hazards associated with the lack of access for Fire Department vehicles. He also noted that the Fire Department recognizes the unique design nature of the New Town development and is prepared to approve this site plan. Ms. Drake said that staff recommended preliminary approval of this site plan subject to comments listed in the staff report that include Nancy's Way alley be extended so as to provide emergency access connections to the adjacent parking lot and New Town Avenue. This would not only help the emergency access, but provide additional safety to the emergency crews responding. Ms. Drake referenced Mr. Horne's May 28th letter to New Town Associates regarding preliminary approval of this residential section did not set a precedent for automatic preliminary approval of future residential sections. Mr. Bob Cosby, the applicant, confirmed that he had received a copy of the letter and added that some of the referenced solutions to provide emergency services for future sections were included in this section. Mr. McCleary and Mr. Fraley discussed generally the concept of New Town alleys and the evolution of the firefighting. Ms. Drake noted that accessory apartments were not permitted over the garages which helped to mitigate the life safety emergency service issues for the Fire Department. Mr. Hill verified for Mr. McCleary that the additional hydrants would allow the fire engine hoses to reach the houses in cases of emergency. There being no further discussion and following a motion by Mr. Hunt that Mr. Fraley seconded, the DRC voted unanimously to recommend preliminary approval be issued for this site plan subject to staff's recommendations detailed in the June 2nd staff report.

CASE NO. S-38-04. GREENSPRINGS WEST, PHASE 4B AND 5

Ms. Drake presented the staff report stating that the plan proposed the next two phases of construction in Greensprings West and that DRC review was required because more than fifty lots are proposed. Staff recommended approval of the plans subject to agency comments being addressed, including the recently received VDOT comments that were distributed to the applicant. Mr. McCleary verified with the applicant, Mr. Jim Bennett and Mr. Rick Smith of AES, that the proffered trails would be constructed. Ms. Drake added that the site plan for the adjacent golf course was currently under review and staff was working with both applicants regarding the trail construction. There being no further discussion and following a motion by Ms Wildman and seconded by Mr. Fraley, the DRC unanimously voted to recommend preliminary approval be issued subject to agency comments being addressed.

CASE NO. SP-56-04 / S-37-04. MICHELLE POINT

Mr. Anderson presented the staff report stating that the Michelle Point site plan and subdivision construction plans require DRC review for three reasons – review of entrance features and signs within the variable width buffer adjacent to Route 30, review of proposed recreation facilities, and because the project exceeds 50 residential units. Staff recommended approval of the entrance feature and sign. Ms. Wildman noted the attractiveness of the sign and the DRC approved its design and location. Staff recommended approval of the proposed recreation facilities. The DRC approved the proposed facilities. Staff recommended deferral of preliminary approval due to two outstanding environmental issues. Scott Thomas of the Environmental Division noted that further analysis was needed for perennial streams and further work needed to be done regarding the 10-point stormwater management system. Darryl Cook of the Environmental Division noted that the Chesapeake Bay Board would not be able to act on an exception required to approve the location of a BMP until August. Mr. McCleary asked whether a deferral to the July 7th DRC meeting would be adequate. Mr. Cook stated that, given they are provided the necessary information prior to the July 7th DRC meeting, the Environmental Division would be comfortable recommending preliminary approval prior to action by the Chesapeake Bay Board. There being no further discussion, the case was deferred to the July 7th DRC meeting.

CASE NO. 59-04. NORGE NEIGHBORHOOD

Ms. Weisiger stated that the applicant had requested a deferral of consideration of preliminary approval for the site plan until the next DRC meeting on July 7, 2004. Mr. Hunt asked why staff had recommended against preliminary approval in the staff report. Scott Thomas explained that proffered Environmental protections had not been clearly met. Mr. Fraley asked how the development had impacted the proffered conservation easement. Ms. Weisiger stated that because the applicant had not shown the easement on the site plan it was not clear, but that it appeared that the applicant had proposed grading into the easement in order to construct buildings. Joyce Lawrie and Gwen McCrae, property owners adjacent to the proposed development expressed concerns about the proposed sidewalks that would be located near their houses and potential problems with skateboarders. They were also concerned that vehicles would use their driveways to turn around. Ms. Weisiger stated that the sidewalk and driveway were owned by the developer and were part of the rezoning proffer agreement approved by the County. Mr. Rogers noted that the rezoning case had been advertised and public hearings had been held for the project. Ms. Weisiger suggested that a meeting could be set up with the developer and property owner to explore how the issues could be addressed. There being no further discussion, the case was deferred to the July 7th DRC meeting.

CASE NO. 51-04. DRUID HILLS SECTION D

Ms. Weisiger presented the staff report stating that the site plan was before the DRC because of concerns by adjacent property owners about the development of the road within an

existing right-of-way known as Braddock Court. Ms. Weisiger stated that VDOT had no objections to preliminary approval being granted and the Environmental Director believes that drainage and stormwater impacts are better in this case with a ditch system and reduced street width. Staff had reviewed the site plan and recommended that the DRC grant preliminary approval of the plan subject to agency comments. At the conclusion of the staff report, Leo Rogers stated that the development was not exempt totally from the Chesapeake Bay Preservation Ordinance, but must comply with it to the maximum extent possible.

Mr. Andy Piplico, the property owner, distributed a letter prepared by his engineer, Kenny Jenkins, responding to neighborhood concerns. He explained that he was not changing the lot sizes or right-of-way; he had attempted to avoid environmental impacts with reduced street width and would not be opposed to adding additional sections of roll-top curb to the street; he had applied to impact only 300 square feet of wetlands; a shared driveway would access two of the lots; he will not be impeding the water from flowing in any way; and, no house will sit on 25% slopes. The housing to be built will be in the price range of \$200,000 to \$250,000.

Mr. Scott Reid of 110 Braddock Road questioned the lack of curb and gutters and had concerns about the environmental impact without them and about the appearance of a street in the subdivision without them; he believed the road at 18 feet was too narrow; he had concerns about the development's impact to the Chesapeake Bay; and knew there had been complaints of road washouts in the area; he believed the development would cause more erosion problems; he had been told previously that Braddock Court was located in the wrong spot. He cannot find where the 50 feet width of the road right-of-way is adjoining his property. He would like to see calculations of run-off and would prefer a bridge over the creek.

Ms. Merianne Reid also of 110 Braddock Road questioned why the property was being developed now after thirty years of people trying to develop it, but being unable to and knowing that people were denied in their attempts to do so. She had requested all documents in the case under a Freedom of Information Act.

Dr. Carl Gerhold of 106 Anthony Wayne stated that while there was no problem now from run-off, he was afraid that development of the property would cause more run-off and problems. The area had steep slopes. People have been told that they could not develop it because of requirements under the Chesapeake Bay acts and he would also like access to County documentation with respect to area.

Ms. Mary Lavin of 108 Anthony Wayne stated that she was a long time resident. She was concerned because that whole area had flooded from time to time. She knew a developer had tried to buy 15' from 1 Braddock Court in order to be able to construct the cul-de-sac. The area of Braddock Court had fallen in and a car had gone into the creek. She said that she was in favor of development if it did not encroach on others' property or provide a mosquito haven.

Mr. Darryl Cook, the County's Environmental Director, said that he had reviewed the plan and basically supported what Mr. Piplico had said. There was less environmental impact from a narrower road. Ditches would filter water and allow infiltration more than curb and gutter

construction. There was a discussion about the use of subsurface drainage systems and about the culvert providing a choke point.


Mr. McCleary asked the County Attorney to advise the DRC on their options. Mr. Rogers said that the committee had limited discretion. If the proposal met VDOT, Zoning, Environmental and Subdivision Ordinances the question was not whether the road can be constructed, but how it is constructed. As to the road location question, he stated that it was long thought that the road would have been easier to build had it been platted further from the creek. He knew of no evidence of a surveying error in the neighborhood or that the right of way was different from what is shown on the plat.

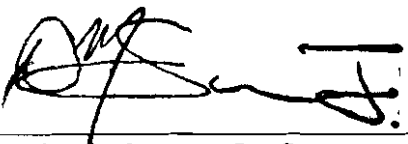
Mr. McCleary put forth a recommendation to defer the case until the next meeting on July 7th and requested that legal documentation be submitted to back up the adjacent property owner claims that the road was in the wrong place. Mr. Fraley said that he empathized with the citizens concerns, but said that it was by-right and going to be developed. He agreed with the recommendation of deferral.

There were further questions from residents about the County's position on the road being constructed and suggestions that the County was changing its standards after not letting others develop the area. There was a discussion of vested rights and uses as it related to this property and the road. Mr. Hunt commented that with the increase in real estate values in the County, infill lots and roads such as this were more likely to be developed. There was a statement that there are many live springs in the area. There was discussion of the type of road construction at 18 feet in width, curb and gutter, and roll top curbs, the level of traffic on the road and possible parking on the shoulder. Mr. Fraley encouraged the property owners to work with the developer. Mr. McCleary proposed to defer the case until the next meeting.

Adjournment

There being no further business, the June 2, 2004, Development Review Committee meeting adjourned at 6:50 p.m.


Mr. Joseph R. McCleary, Chairman


O. Marvin Sowers, Jr., Secretary

Site Plan 056-04/Subdivision Plan 037-04
Michelle Point
Staff Report for the July 7, 2004 Development Review Committee Meeting

SUMMARY FACTS

Applicant: Jay Epstein

Land Owner: Michelle Point, LLC

Proposed Use: Single family and townhouse units.

Location: 9001 Barhamsville Road

Tax Map/Parcel No.: (12-1)(1-3)

Primary Service Area: Inside

Parcel Size: 38.704

Existing Zoning: R-5 Cluster, with Proffers

Comprehensive Plan: Low Density Residential

Reasons for DRC Review: The project exceeds 50 residential units

Staff Contact: Ellen Cook Phone: 253-6685

STAFF RECOMMENDATION

At the last DRC meeting consideration of preliminary approval was deferred in order to allow time to resolve two Environmental Division issues: perennial stream determination, and discrepancies in the BMP point system worksheet. Based on meetings that have occurred between the applicant and the Environmental Division, it now appears that both issues have been adequately resolved subject to the applicant submitting additional information showing compliance with the 10 point system for BMP/Water Quality. As a result, staff now recommends preliminary approval subject to agency comments, as previously included in the June 2, 2004 DRC report, and subject to Environmental Division concurrence.



Ellen Cook

Site Plan 80-04
JCC Communications Tower - Emergency Operations Center
Staff Report for the July 7, 2004 Development Review Committee Meeting

SUMMARY FACTS

Applicant: Mr. Richard Miller, Fire Chief

Landowner: James City County

Proposed Use: 160 foot communication tower serving as part of the JCC 800-MHz trunked radio system.

Location: 3127 Forge Road

Tax Map/Parcel No.: (13-3)(1-27)

Primary Service Area: Yes

Existing Zoning: B-1, General Business

Comprehensive Plan: Federal, State & County Land

Reason for DRC Review: Section 15.2-2232 of the Virginia State Code requires Planning Commission review of any public area, facility or use not shown on the adopted Comprehensive Plan. This code states that no facility shall be allowed unless the commission determines that the location, character and extent of the facility is "substantially" in accord with the adopted Comprehensive Plan.

Staff Contact: Matthew Arcieri, Planner Phone: 253-6685

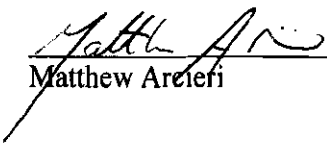
STAFF RECOMMENDATION

The special use permit for this tower was approved by the Board of Supervisors on June 8, 2004.

Staff finds the proposal consistent with the Comprehensive Plan because:

1. By developing a regional radio system with York County, the proposal satisfies goal four of the public facilities element, "Emphasize efficient facilities and service delivery systems and develop public facilities as components of regional systems where feasible."
2. The tower is a public facility owned and operated by James City County on land designated Federal, State & County Land on the Comprehensive Plan.

Staff recommends the DRC find the tower consistent with the Comprehensive Plan.


Matthew Arcieri

Attachments:

1. Tower layout

Site Plan 72-04
JCC - Emergency Communications Center Building
Staff Report for the July 7, 2004 Development Review Committee Meeting

SUMMARY FACTS

Applicant: Mr. Jason Grimes, AES Consulting Engineers

Landowner: James City County

Proposed Use: 7,156 square foot Emergency Communications Center

Location: 3127 Forge Road

Tax Map/Parcel No.: (13-3)(1-27)

Primary Service Area: Yes

Existing Zoning: B-1, General Business

Comprehensive Plan: Federal, State & County Land

Reason for DRC Review: Section 15.2-2232 of the Virginia State Code requires Planning Commission review of any public area, facility or use not shown on the adopted Comprehensive Plan. This code states that no facility shall be allowed unless the commission determines that the location, character and extent of the facility is "substantially" in accord with the adopted Comprehensive Plan.

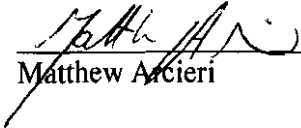
Staff Contact: Matthew Arcieri, Planner Phone: 253-6685

STAFF RECOMMENDATION

During the Special Use Permit process for the tower on this property, several adjacent property owners expressed concern over the visual impact of the EOC expansion on their property. The site plan not only preserves the existing tree buffer but also adds a fully landscaped 35 foot transitional buffer between the new building and adjacent properties.

Planning, VDOT and the applicant are working on resolving issues over the temporary parking area to ensure safety for vehicles and pedestrians and to ensure it does not negatively impact, on a temporary basis, the Forge Road Community Character Corridor.

Staff finds the proposal consistent with the Comprehensive Plan because the new building is a public facility owned and operated by James City County on land designated Federal, State & County Land on the Comprehensive Plan. Staff recommends the DRC find the ECC Building consistent with the Comprehensive Plan.


Matthew Arcieri

Attachments:

1. Agency Comments
2. Location Map
3. Site Plan (Under Separate Cover)

AGENCY COMMENTS

Planning:

1. This plan will be reviewed by the Development Review Committee on July 7, 2004 at 4PM in accordance with section 15.2-2232 of the Virginia State Code.
2. Please revise the tax map number to (12-3)(01-0-0027).
3. Please include the JCC Case No. SP-72-04.
4. In accordance with Section 24-98, mechanical yards shall be screened from adjacent residential districts. Please replace the chain link fence with a solid wooden enclosure.
5. How will handicapped parking be provided to the existing EOC building during construction?
6. The temporary parking area is located in the 50' right-of-way buffer for Forge Road. A landscape modification can be granted for the location provided effective screening from the road, as approved by the Director of Planning, is proposed. Please also consider moving the area further south and out of the buffer.
7. A landscape modification is required to locate the service drive in the rear landscape buffer and to reduce the amount of building perimeter plantings.

VDOT:

1. Please see the attached comments.

JCSA:

1. Please see the attached comments.

Environmental:

1. Comments are outstanding and will be forwarded when received.



COMMONWEALTH of VIRGINIA

DEPARTMENT OF TRANSPORTATION

4451 IRONBOUND ROAD
WILLIAMSBURG, VA 23188

PHILIP SHUCET
COMMISSIONER

June 24, 2004



STEVEN W. HICKS
RESIDENT ENGINEER
TEL (757) 253-4832
FAX (757) 253-5148

Matt Arcieri
James City County Planning
Post Office Box 8784
Williamsburg, Virginia 23187

Ref: ECC Building
SP-072-04
Route 60, James City County

Dear Mr. Arcieri:

We have completed our review of the above mentioned site plan and offer the following comments:

- ◆ We recommend that stop signs (MUTCD R1-1, 30" x 30") and stop bars (24" width) be placed at all entrances, if such does not already exist.
- ◆ We do not recommend that the "temporary gravel parking area" be allowed along Forge Road (Route 610), due to inadequate sight distance at the existing gravel entrance.

When the above comments have been addressed, please submit two sets of revised plans to this office for further review. Also, attach a letter noting what action was taken to correct the above comments and any revisions that may impact the right-of-way.

Should you have any questions please contact me at 253-4832.

Sincerely,

Anthony L. Handy, PE, LS
Assistant Resident Engineer



MEMORANDUM



Date: June 25, 2004

To: Matthew Arcieri, Planner

From: Timothy O. Fortune, P.E. - Civil Engineer

Subject: SP-072-04, ECC Building (Construction plans)

James City Service Authority has reviewed these plans for general compliance with the JCSA Standards and Specifications, Water Distribution and Sanitary Sewer Systems and have the following comments for the above project you forwarded on June 4, 2004. Quality control and back checking of the plans and calculations for discrepancies, errors, omissions, and conflicts is the sole responsibility of the professional engineer and/or surveyor who has signed, sealed, and dated the plans and calculations. It is the responsibility of the engineer or surveyor to ensure the plans and calculations comply with all governing regulations, standards, and specifications. Before the JCSA can approve these plans for general compliance with the JCSA Standards and Specifications, the following comments must be addressed. We may have additional comments when a revised plan incorporating these comments is submitted.

General Comments:

1. The plan shall be reviewed and approved by the James City County Fire Department. Per JCSA standards and specifications Section 2.11 for this type of development, a fire flow demand of 2500 gpm is required. Applicant shall confirm the existing JCSA water system will provide the fire flow volume and duration as specified by the JCC Fire Department and/or make necessary improvements to the existing water system to meet those requirements. Any fire flow other than that listed above must be approved by the JCC Fire Department with appropriate documentation submitted to JCSA for verification.
2. Add the following note to the plans: "The plumbing inside the building must be inspected by Mr. John Wilson, JCSA Utility Projects Special Coordinator at (757) 259-4138, for potential cross connections. Any cross connections must be protected by the appropriate backflow prevention device(s)".

Sheet C-1:

1. General Notes: Revise note 10 to read as follows: "Any existing unused wells shall be abandoned in accordance with State private Well Regulations and James City County Code".

Sheet C-3:

1. Based on JCSA records, a water meter currently serves the existing recycling center which is planned for demolition. The Applicant shall indicate on the plans the water meter to be removed. Provide a note on the plan which requires the abandoned water service line for the recycling center to be disconnected at the JCSA main. This shall involve removal of the service saddle and installing a full circle stainless steel repair band on the existing water main.

Sheet C-4:

1. Clearly indicate the connection requirements to all existing manholes (cored opening, kor-n-seal boot with invert reshaped).
2. Plan describes use of a flat top slab manhole w/IC-2 for the sanitary sewer manhole replacement. Provide a detail or clearly reference IC-2 as a VDOT detail. Add a note on the plan requiring lettering of the manhole cover to read "SEWER". The applicant shall research if a frame is available for the flat top slab unit which will accommodate HRPDC detail SS_10 standard cover.
3. Graphically show the proposed water meter as being installed within the existing right-of-way line. Revise accordingly.
4. The Applicant shall verify with the building plumbing design if the proposed cleanout invert elevation can be raised to elevation 98.5 (+/-). If so, this would eliminate the need for the proposed sanitary manhole adjacent to the building and allow the 6-inch lateral to extend, at 1% slope, to the proposed manhole at the entrance.
5. Due to minimal depth, revise the proposed 84 LF segment of sanitary sewer replacement from "PVC" to "DIP".
6. The section of sanitary sewer pipe beneath the proposed entrance shall be replaced with ductile iron pipe from the replaced manhole to a minimum of 40 lf upstream. Specify on the plan the type of transition coupling (manufacturer and model) to be used.
7. Label the size of all existing water mains.
8. It appears that the "Future Building" location shown at the southeast corner of the parcel conflicts with the existing water mains as well as overlaps into the JCSA Well site property. The Applicant shall note that review and approval of these plans in no way incorporates the future layout features for this site which will be addressed separately at that time.
9. The proposed dumpster pad and enclosure shall have a minimum horizontal clearance of 5-feet from the existing fire hydrant water line. The water line shall be field located prior to approval of these plans (refer to Sheet C-5, Comment #1 below). Revise plan accordingly to comply.

10. Clearly label on the plans the type of pipe to be used for the water service connection within the right-of-way.

Sheet C-5:

1. The Applicant shall field locate, both horizontally and vertically, the utility conflict between the existing fire hydrant water line and the proposed 12-inch storm sewer line. The note provided for coordination with JCSA is unacceptable. Clearly define on the utility plan the requirements for offsetting the existing water main, if required.

Sheet C-7:

1. HRPDC/JCSA Detail List:
 - a. Add JCSA detail W14.0 meter setting detail to the list.

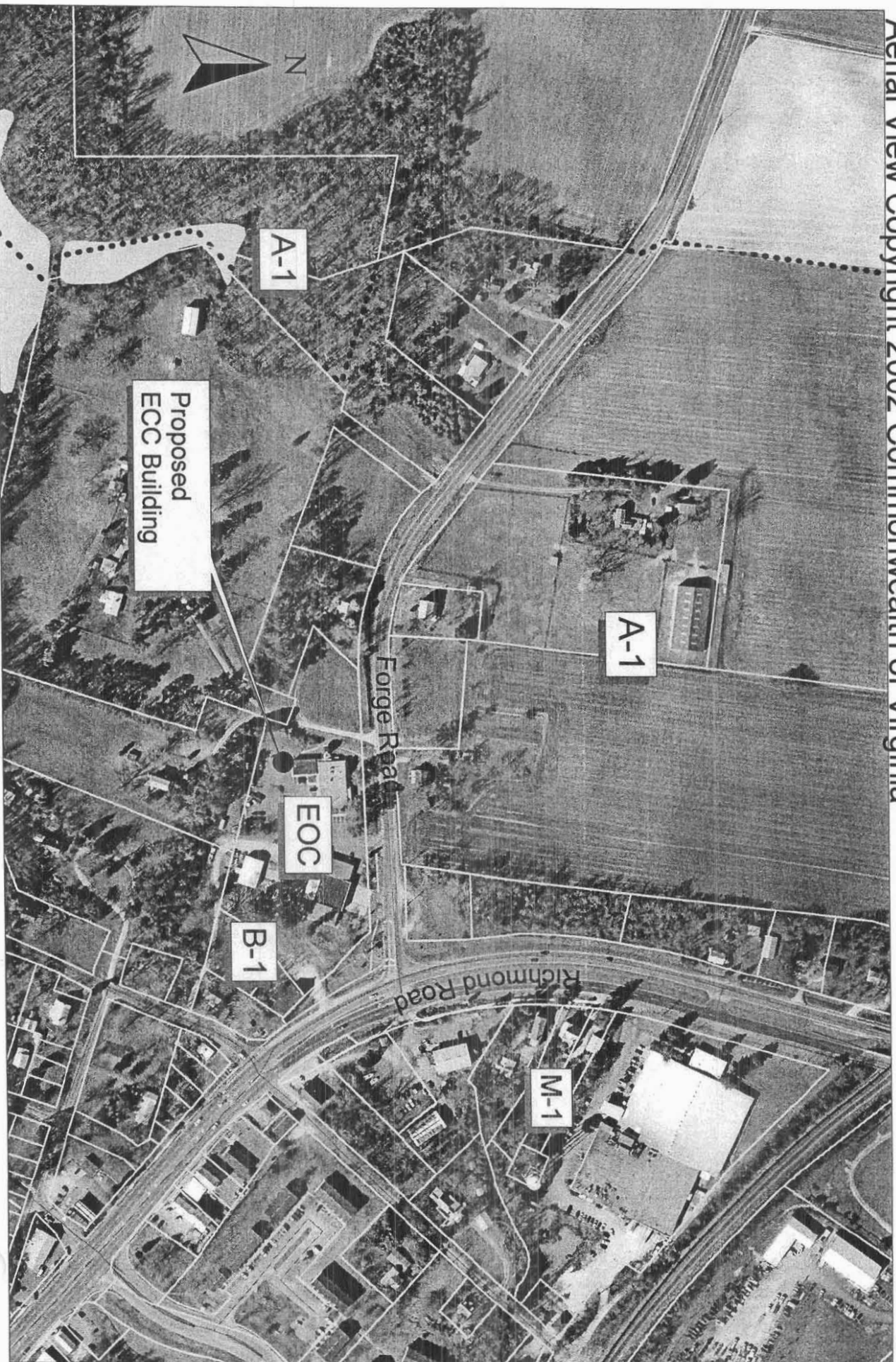
Sanitary Sewer System Data Sheet:

1. Revise the data sheet to reflect comments provided above.
2. Section 6: Revise the 84' of sewer replacement to reflect DIP. Refer to Sheet C-4 Comment #4.

Please call me at 253-6836 if you have any questions or require any additional information.

SP-72-04, JCC Emergency Communications Center (Forge Road)

Aerial View Copyright 2002 Commonwealth of Virginia



Site Plan 69-04

New Town - Block 5, Parcels D and E Mixed Use Buildings

Staff Report for the July 7, 2004, Development Review Committee Meeting

SUMMARY FACTS

Applicant: Robert Cosby, AES Consulting Engineers

Land Owner: GCR, Inc. and Atlantic Homes

Proposed Use: Approval of four mixed use buildings:

Building A-1 shares a common wall with the Corner Pocket and will contain 3,855 square feet of retail space and two residential units.

Building A contains 4,381 square feet of retail and contains six residential units.

Building B contains 4,038 square feet of retail space and eight residential units.

Building C contains 7,336 square feet of retail, 7,183 of commercial square footage and six residential units.

Location: New Town - 5101 and 5109 Center Street

Tax Map/Parcel No.: (38-4)(24-7) and (38-2)(24-1)

Primary Service Area: Inside

Parcel Size: 1 acre

Existing Zoning: MU, Mixed Use, with Proffers

Comprehensive Plan: Mixed Use

Reason for DRC Review: The development proposes buildings whose floor area exceeds 30,000 square feet.

Staff Contact: Matthew Arcieri Phone: 253-6685

STAFF RECOMMENDATION

The New Town Design Review Board reviewed and approved these buildings on June 17, 2004. Staff recommends the DRC grant preliminary approval subject to agency comments.


Matthew D. Arcieri

Attachments:

1. Agency comments
2. Location Map
3. Site Plan (Under Separate Cover)

AGENCY COMMENTS

Planning:

1. This plan proposes more than 30,000 square feet and will be reviewed by the Development Review Committee on July 7, 2004 at 4PM.
2. On the cover sheet, please note the date this site plan was approved by the New Town DRB.
3. Final tax map number and addresses for these properties have been provided by real estate. The correct map number for parcel D is (38-4)(4-7) and its address is 5101 Center Street. The correct map number for parcel E is (38-2)(24-1) and its address is 5109 Center Street. Please revise that plans accordingly.
4. Will all residential units be for sale? This information is necessary for real estate to properly address units.
5. Although not required by ordinance, staff recommends a bike rack be located near the vicinity of the entrance to buildings A and B.
6. Prior to final approval community association documents must be submitted to the County Attorney for review and approval in accordance with the proffers.
7. Prior to final approval, the water source cash contribution shall be required. This should be made payable to the James City Service Authority. Please note that the per lot amount must be adjusted by the CPI in accordance with the proffers. Please submit these calculations to John McDonald for approval.
8. Prior to final approval, the school construction contribution shall be required. This should be made payable to Treasurer - James City County. Please note that the per lot amount must be adjusted by the CPI in accordance with the proffers. Please submit these calculations to John McDonald for approval.

Landscaping:

1. A landscape modification to Sec. 24-95 of the James City County Zoning Ordinance is required as the proposal does not meet minimum requirements for building perimeter landscape area or quantity.

JCSA:

1. Please see the attached comments.

Environmental:

1. Comments are outstanding and will be forwarded when received.

VDOT:

1. Please see the attached comments.



MEMORANDUM



Date: June 25, 2004

To: Matt Arcieri, Planner

From: Shawn A. Gordon, P.E. - Project Engineer

Subject: SP-069-04, New Town Block 5, Parcels D and E, Mixed Use Office Buildings

James City Service Authority has reviewed these plans for general compliance with the JCSA Standards and Specifications, Water Distribution and Sanitary Sewer Systems and have the following comments for the above project you forwarded on June 3, 2004. Quality control and back checking of the plans and calculations for discrepancies, errors, omissions, and conflicts is the sole responsibility of the professional engineer and/or surveyor who has signed, sealed, and dated the plans and calculations. It is the responsibility of the engineer or surveyor to ensure the plans and calculations comply with all governing regulations, standards, and specifications. Before the JCSA can approve these plans for general compliance with the JCSA Standards and Specifications, the following comments must be addressed. We may have additional comments when a revised plan incorporating these comments is submitted.

General Comments:

1. Replace General Note #7 with the following note: "Any existing unused wells shall be abandoned in accordance with State Private Well Regulations and James City County Code."
2. This site plan does not appear to show how the proposed buildings will be served by public sanitary sewer. Show and label the proposed sanitary sewer connection to the JCSA sanitary sewer system. All laterals beyond the existing JCSA clean-outs should be labeled as "private". In addition, label the lateral pipe inverts at the connections.
3. Building "A-1" does not appear to have a water meter proposed nor was it included in the meter sizing calculations. If Building "A-1" has an individual water meter, water demand calculations (based on fixture units) in the proposed building shall be submitted for meter and service line sizing verification. The plans submitted do not appear to correspond with the meeting held June 2, 2004 between GCR, Inc., AES Consulting Engineers, and JCSA regarding gang metering for the proposed mixed use buildings. Verify and/or state the layout discussed during the June 2, 2004 meeting will not be used for this project.

4. It appears the proposed Building "A" is the only building proposed to have a fire suppression as currently shown. Verify and/or provide explanation.
5. The plans should be submitted to the James City County Fire Department for verification the existing fire hydrant along Center Street is acceptable due to the close proximity to proposed Building "B".
6. JCSA does not recommend a C-Factor of 150 when sizing water service lines. A more acceptable industry standard of C = 130 is recommended.

Sheet 4:

1. The proposed 6" double detector check valve vault with siamese connection is shown incorrectly. The siamese connection should be upstream of the detector check valves in accordance with JCSA standards. Revise accordingly.
2. Provide joint restraint for the water main extension to the proposed detector check vault serving proposed Building "A".
3. Provide clarification of the proposed water meter "WD_15" detail references. The HRPDC Regional Standards does not include a "WD_15" detail. Was the intent to reference the JCSA 1 ½" & 2" Water Meter Setting Detail, W15.0? Verify and revise accordingly.
4. Provide clarification of the proposed water meter "WD_17" detail references. The HRPDC Regional Standards does not include a "WD_17" detail. Was the intent to reference the JCSA Double Gate, Double Check Detector Check Assembly Detail, W17.0? Verify and revise accordingly
5. The proposed water meters should be within the JCSA Urban Easement not centered on the easement as shown. Revise plan per JCSA standards.
6. JCSA Notes: Revise the portion of Note #2 stating "*inspected by JCSA Operations at (757) 229-7421*" to "*inspected by Mr. John Wilson, JCSA Utility Special Projects Coordinator at (757) 259-4138*".

Sheet 5:

1. The proposed 6-inch storm drain pipe appears to be in conflict with the existing fire hydrant located on the northern side of Building "B" along Center Street. Verify and revise accordingly.

Water Data Sheet:

1. Provide clarification whether proposed Building "A-1" was included in the domestic average and peak flows calculated.

Sanitary Sewer Data Sheet:

1. Provide clarification whether proposed Building "A-1" was included in the domestic average and peak flows calculated.

Please call me at 253-6679 if you have any questions or require any additional information.



COMMONWEALTH of VIRGINIA

DEPARTMENT OF TRANSPORTATION

4451 IRONBOUND ROAD
WILLIAMSBURG, VA 23188

PHILIP SHUCET
COMMISSIONER



STEVEN W. HICKS
RESIDENT ENGINEER
TEL (757) 253-4832
FAX (757) 253-5148

June 21, 2004

Matthew Arcieri
James City County Planning
Post Office Box 8784
Williamsburg, Virginia 23187

Ref: New Town Block 5, Parcels D & E, Mixed Use Office Buildings
SP-069-04
Monticello Avenue (Route 321), James City County

Dear Mr. Arcieri:

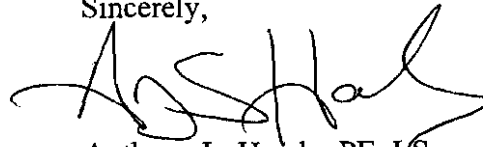
We have completed our review of the above mentioned site plan and offer the following comments:

- ◆ VDOT Right of Way is not clearly defined on sheet 4, 5 and 6. Ensure that proposed building C is located outside of VDOT Right of Way.
- ◆ VDOT will not be responsible for any site improvements, such as plantings, lighting, sidewalk, steps, railings, patio, etc., shown on the plans. Will any of these improvements be located within the Right of Way? It must be clearly noted on the plans that VDOT will not be responsible for maintenance and will be held harmless. A maintenance agreement will also be required for any such improvements located within the VDOT Right of Way.
- ◆ It appears from the plans that the building rooftop and site drainage is being discharged directly into the VDOT Drainage system via 6" HDPE pipes. The VDOT drainage structures should be tied into with a minimum 12" HDPE pipe, with inverts more closely matching existing inverts of drainage structures. VDOT will not be responsible for site drainage system or connections. A note must be added to the plans stating such.

New Town Block 5, Parcels D & E, Mixed Use Office Buildings
June 21, 2004
Page Two

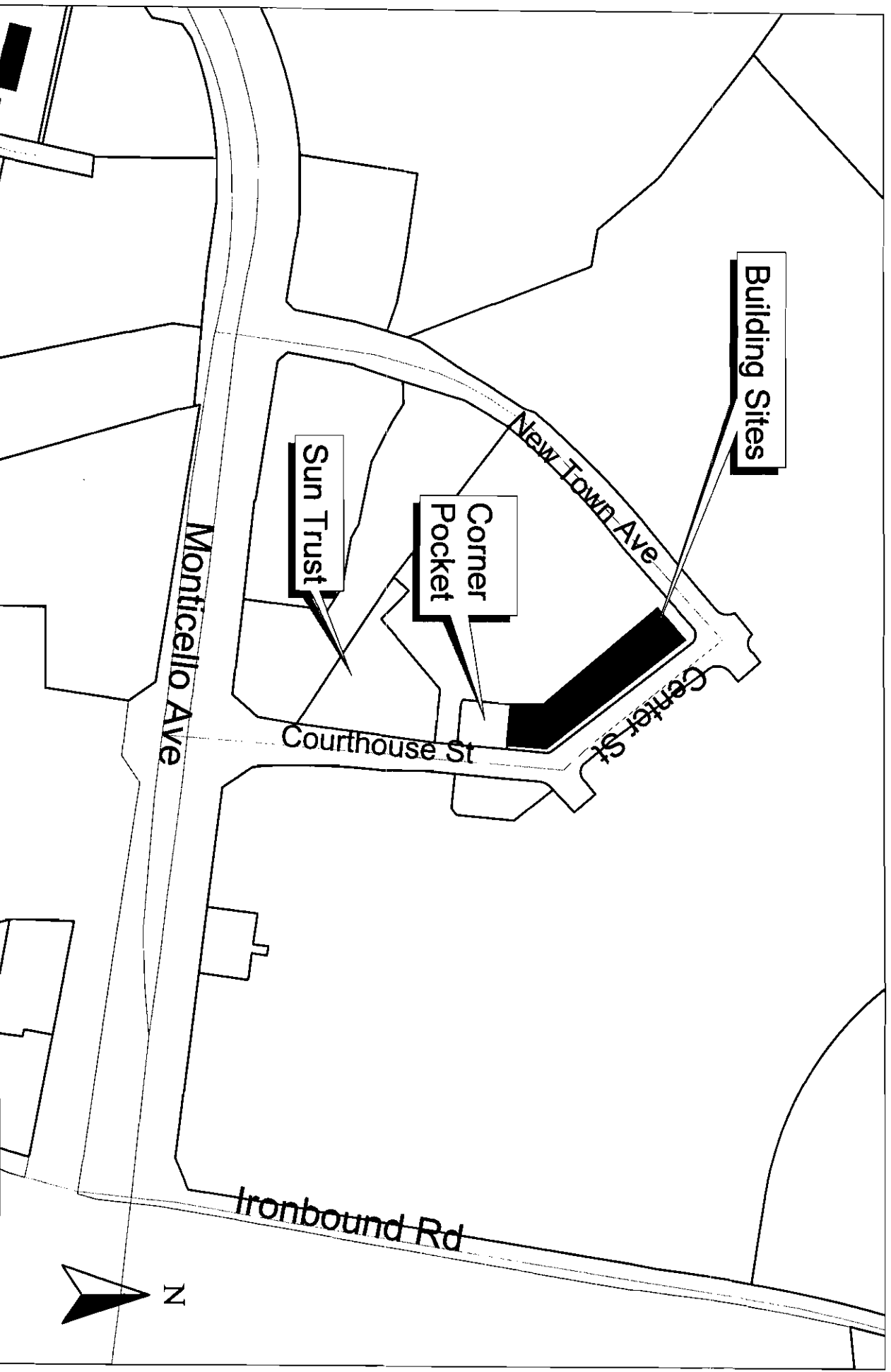
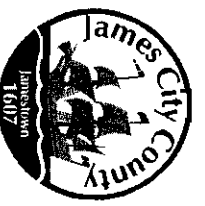
Should you have any questions please contact me at 253-4832.

Sincerely,

A handwritten signature in black ink, appearing to read 'ASHa', with a stylized, sweeping flourish extending from the end.

Anthony L. Handy, PE, LS
Assistant Resident Engineer

SP-69-04, New Town - Block 5, Parcels D & E Mixed Use Buildings



Case No. C-07-03
New Town: Town Center Parking Overview
Staff Report for the July 7, 2004 Development Review Committee Meeting

Summary Facts:

Applicant: Larry Salzman
Land Owner: New Town Associates

Proposed Use: Mixed Use (*Primarily Commercial & Residential*)

Location: New Town Section 2 & 4: Town Center
Block 2 (*William E. Wood Building*)
Block 5 (*SunTrust Building/Corner Pocket*)
Block 8 (*Residential*)

Tax Map/Parcel: (38-4)(1-50)

Primary Service Area: Inside
Parcel Size: ±86 Acres

Existing Zoning: Mixed Use with Proffers
Comprehensive Plan: Mixed Use: New Town

Reason for DRC review: To allow for general off-site parking and shared parking for all of Block 2, 5 and 8 and begin automatic review of off-site and shared parking at New Town.

Staff Contact: Karen Drake---(757) 253-6685

Staff Recommendation:

At the February 25th Development Review Committee Meeting the DRC recommended off-site parking, shared parking and waived the minimum off-street parking requirements per the Zoning Ordinance as long parking provided is accordance with the New Town Design Review Guidelines for Block 2 and Block 5 of New Town. Development plans for Block 8 are now under review and the shared parking calculations for Block 8 are presented for the first time.

Additionally, the DRC approved a block by block parking quarterly parking review based on the conditions listed below ***with staff comments in bold italics***:

1. New Town Associates establishes and updates a chart and accompanying site layout plan that details building square footage and use, Zoning Ordinance parking requirements, New Town Guidelines Parking Requirements, shared parking methodology and details the number of parking spaces allocated on-site and off-site. The chart should be structured in such a manner that illustrates that off-site parking is not allocated multiple times. The chart and accompanying site layout plan would be submitted for review and approval on a quarterly basis by staff and the Development Review Committee via the consent calendar. A quarterly review will allow for new lease negotiations to develop, construction of buildings and verification that the off-site and shared parking methodology is realistically working. DRC approval would be issued for a block by block waiver of parking requirements and to permit off-site parking.

The July 2004 quarterly update is attached for your review. Staff finds the parking calculations satisfactory.

2. A letter is submitted for review and approval by the County Attorney and shall be added to the attached parking overview that documents the permanent availability of the off-site and shared parking.

The New Town Owner Association Documents which have been reviewed and approved by the County Attorney addresses the permanent availability of the off-site parking.

3. Any change by New Town Associates to the shared parking methodology in the attached report on basic parking overview will be approved by the DRC at a quarterly review.

No change in methodology from February.


4. If at any time New Town Associates does not responsibly update the master chart on a quarterly basis or the DRC does not find the updated parking figures acceptable, off-site parking review shall revert back to an individual building basis.

To date, this condition has been met.

5. In July of 2005, New Town Associates will conduct a study of the overall New Town parking supply and demand for the DRC to review and approve. In addition to evaluating this study, the DRC will review how frequently this overall study needs to be conducted, evaluate the entire parking review process and make any changes as necessary.

Not applicable at this time.

Staff recommends the DRC approve the July 2004 New Town Shared Parking update with the October report to be placed on the consent agenda.



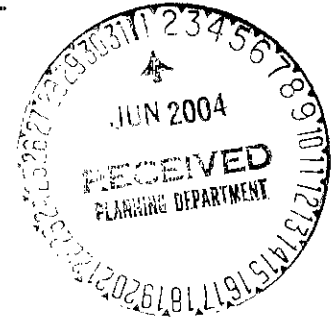
Karen Drake
Senior Planner

Attachments:

- 1.) June 1st New Town Shared Parking Update for Blocks 2 & 5
- 2.) June 22nd New Town Shared Parking for Block 8

NEW TOWN

There's A New Revolution In Williamsburg, Virginia.™



June 1, 2004

By Hand

Ms. Karen Drake
Senior Planner
James City County, Virginia

RE: JCC Case No. C-07-03 New Town Shared Parking

Dear Karen:

At the March 1st meeting the JCC Planning Commission approved the Development Review Committee's recommendation to approve off-site parking, shared parking and waive the minimum off-street parking requirements per the Zoning Ordinance as long as parking provided is in accordance with the New Town Design Review Guidelines for Block 2 and Block 5 of New Town, Section 2 & 4.

The Planning Commission also approved the Development Review Committee's recommendation for a block by block parking review process. The process included submission of a chart that could be updated on a quarterly basis which demonstrates the shared parking plan in New Town.

Attached are three charts with supporting text. The first chart demonstrates the parking for Block 5, the second chart is Block 2, and the third chart provides an overview of the town center for New Town and shows the location of Block 2 and Block 5.

Although the uses for Block 5 are now determined, construction of the central parking lot is not quite 100% complete. Accordingly, the chart is not quite final. The entry from New Town Avenue into the parking lot (closest to Monticello Avenue) took longer to construct than anticipated due to the location of a manhole. Slight revisions to the parking in its final form are likely, due to slight changes in construction, slight changes in locations of dumpsters, and other minor on-the-ground variations. The chart does demonstrate the uses and requirements for parking and suggests that the parking is in line with what was approved. Once construction is complete, which we expect to occur in the next 2 to 3 weeks, I will have a new chart prepared and will show the exact on-the-ground conditions and demonstrate the parking for Block 5.

Ms. Karen Drake
June 1, 2004
Page 2

Construction of Block 2 is also nearing completion, but the attached chart is not final. As you know, we have agreed that the parking near the entry closest to Monticello Avenue will be changed to angle parking rather than straight-in parking. Our recent meeting between users, New Town Associates, James City County, and VDOT indicated that angled parking would be better. Since the on-the-ground construction is not quite complete I don't know the exact number of parking spaces in this block (there may be other slight on-the-ground variations due to dumpster locations or actual construction conditions). The chart which is attached demonstrates that shared parking does work. Once again this is not a final plan and slight revisions will be made.

I was hopeful that by this date I would be submitting a plan for parking in Block 8. Unfortunately we are working on a revision to one of the uses for this block and I am not quite ready to submit Block 8 for approval.

As you probably know we are also working with a theater and retail developer for Main Street. We have started to work on the parking analysis for the theater and Main Street however it is in very preliminary form at this time. Blocks 2 and 5 begin to show how the theater parking may work. Block 2 and Block 5 are dominated by office uses which have peak parking at 2 P.M. on a weekday. These uses, and the parking associated with them, will share well with a theater which has peak uses in the evening and on weekends. I am working on this parking analysis and I am hopeful that 30 days from now we can provide a preliminary view of this parking as well.

I hope that the attached charts, approximating parking for Blocks 2 and 5 (the third chart shows their locations in New Town) are adequate to meet your requirements as of this date. I expect that within 30 days construction of Block 2 and Block 5 will be complete and that I can provide charts for those two blocks which demonstrate the actual conditions. I am also hopeful that in 30 days I can provide at least a preliminary plan for the parking in Block 8, as well as some indication of how the parking for the theater and Main Street development will work.

Let me know if you have any additional requirements or would like additional information prior to the next meeting of the DRC. Thank you.

Sincerely,
New Town Associates, LLC



Lawrence Salzman
Managing Director

**New Town
Block 2
Parking Report
May 2004**

Block 2 fronting on Monticello Avenue and across Courthouse Street from the SunTrust block is currently under construction. Slight revisions will occur. The parking at the entrance near Monticello Avenue will be changed to angle parking. Other minor changes due to dumpster locations or field conditions may also occur. Some uses in Block 2 are known and others are projected. An analysis of Block 2 uses and parking requirements is as follows:

		<u>Block 2</u>			
<u>Use</u>		<u>Size</u>	<u>Parking Demand</u>	<u>2 P.M. Ratio</u>	<u>2 P.M. Demand</u>
W. E. Wood	Office	21,400*	77	.97	75
AHLM	Office	17,000*	61	.97	59
	Retail	1,900	7	.97	7
DCI	Office	15,000*	54	.97	52
Contract	Office	4,000*	14	.97	14
	Retail	4,000	15	.97	15
Retail	Unknown	58,000	221	.97	<u>215</u>
Total					437

* Office is based on approximately 90% usable efficiency for typical buildings

The design of the Block 2 central lot provides 374 spaces (including handicap spaces) plus about 65 spaces will be provided on Courthouse Street and Main Street for a total of 439 spaces. Supply in the block of 374 spaces complies with the design guidelines. Shared parking demand (437) is in line with supply (439) and saves about 110 spaces as compared to stand alone buildings.

Block 2 Summary

<u>Use</u>	<u>Size</u>	<u>Peak Shared Parking Demand</u>	<u>Market Demand</u>	<u>Design Guidelines</u>
Office	57,400	200	230	173
Retail	63,900	<u>237</u>	<u>320</u>	<u>256</u>
		437	550	429

Block 2 will be about half office and half retail. Current plans suggest that parking supply and demand will be in balance. Slight revisions may be required as final uses are determined.

**New Town
Block 5
Parking Report
May 2004**

Block 5 (the block where the SunTrust Bank building and the Corner Pocket are now opened) will consist of the 60,000 square foot SunTrust building, the 11,080 square foot Corner Pocket (about half office and half retail), additional retail of approximately 17,400 feet, additional office of about 9,400 square feet, 22 apartments, and 19 attached and detached for sale residential units, each with a one or two car garage.

Using the shared parking concept, office uses are calculated at four spaces per 1,000 of usable square feet, retail at 3.8 spaces per 1,000 square feet of floor area and residential uses at 1.5 spaces per unit. At the 2 P.M. peak hour office and retail uses are calculated at 97% of the above stated rates (based on Urban Land Institute studies) and residential uses at 55% of the rate. The overall parking calculation for the block would therefore be as follows:

Block 5					
Use		Size SF or du	Parking Demand	2 P.M. Ratio	2 P.M. Demand
SunTrust	(Office)	60,000*	216	.97	210
Corner Pocket	(Retail)	5,380	20	.97	19
	(Office)	5,700*	21	.97	20
AHLM	(Retail)	7,336	28	.97	27
	(Office)	7,183*	26	.97	25
	(Apts)	6	9	.55	5
GCR	(Retail)	10,069	38	.97	37
	(Office)	2,250*	8	.97	8
	(Apts)	16	24	.55	13
Homes for Sale		19**	15	.55	<u>8</u>
Total					372

*Office is based on approximately 90% usable efficiency for typical buildings.

**The homes for sale will consist of 12 townhouses, each having a two car garage, and 7 carriage houses, each having a one car garage. At least 4 of the carriage houses will have one adjacent paved parking space in addition to the one car garage. We are allocating an additional one space per unit for the 15 units without additional on site parking.

Total peak parking demand for Block 5 would therefore be 372 spaces. Parking supply is 325 spaces in the central lot, at least 4 paved spaces next to the carriage houses and approximately 39 spaces will be available on Courthouse Street, Center Street, and New Town Avenue for a total of 368 spaces. There are also 31 garages which are not counted in the supply because they won't be shared with other users.

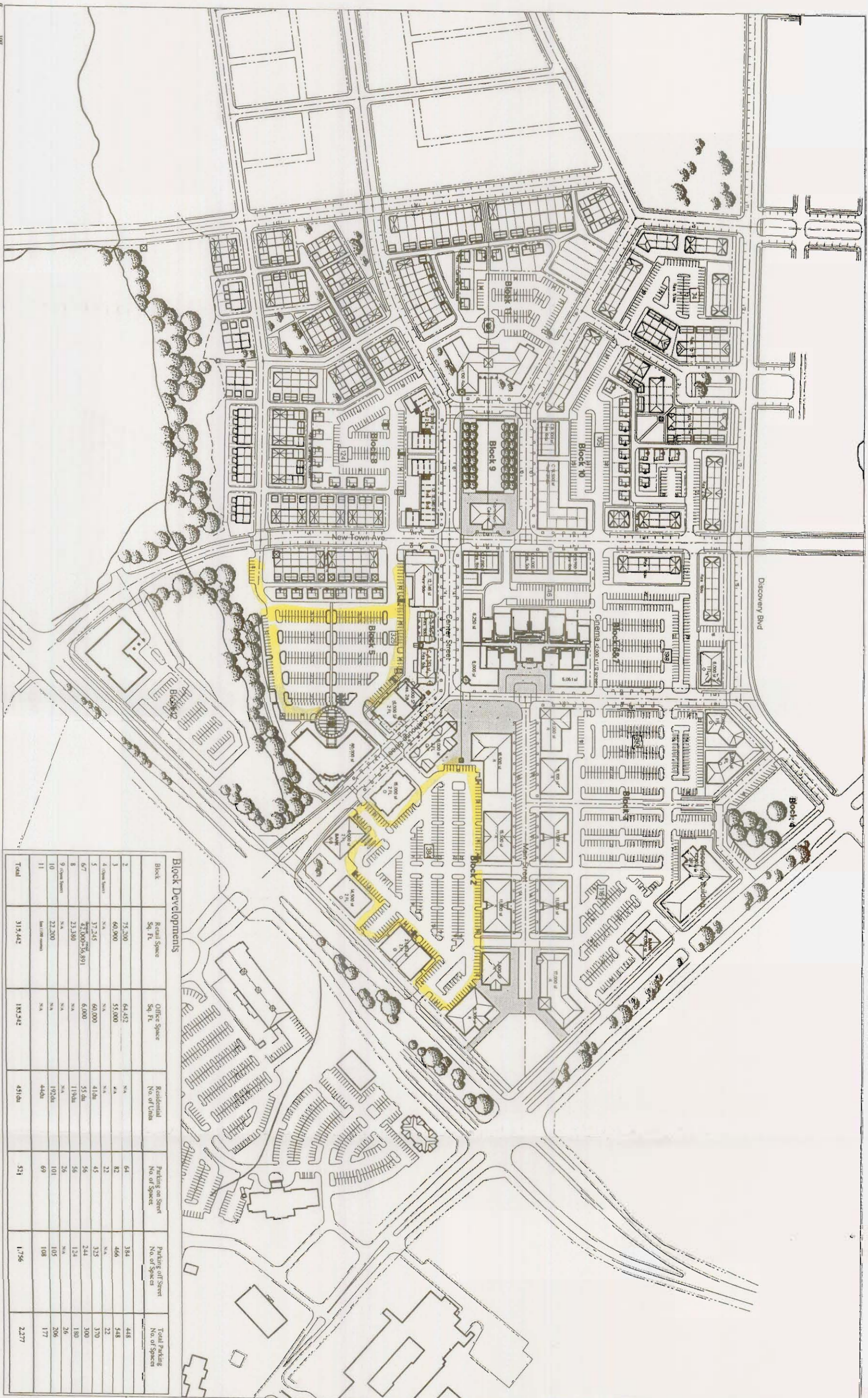
The largest use in Block 5 will be office with about 67,600 square feet of usable space. This office space requires approximately 263 spaces at peak demand, and these spaces will primarily be used between 8:30 A.M. and 5:00 P.M. on weekdays. On evenings, weekends, and holidays these spaces will be essentially unoccupied. This block will also have about 22,800 feet of retail space, including the Corner Pocket restaurant. The busy times for the Corner Pocket restaurant will be lunch (some of the customers will consist of office workers nearby) and after 5 P.M. when the office spaces are not being heavily utilized. At the peak demand time of 2 P.M. on a weekday the pool tables at the Corner Pocket have very few users. This block also includes 22 apartments which will require about 33 parking spaces and 19 single-family homes for sale. Twelve of these homes will have a two car garage and seven homes will have one car garages. At least four of the carriage houses will also have an adjacent paved parking space. The 41 residential units will share well with the dominant office uses in this block. Traditional market demand (4/1000 office, 5/1000 retail, 1.5 per unit) would total about 446 spaces. Design guidelines parking (3/1000 office, 4/1000 retail, 1.5 per unit) would indicate a maximum of about 357 spaces, and the block parking provided (including lined spaces but not garages) is 329 spaces. This indicates that about 117 fewer spaces (86 if you count garages) are required than for free standing buildings without shared parking. Overall this block seems to demonstrate that a mix of uses, utilizing a shared central

parking lot, garages and some on street parking, will all have adequate parking even at the times of their peak demand, and the overall community benefits by having less parking than if these were stand alone buildings.

Block 5 Summary

<u>Use</u>	<u>Size</u>	<u>Peak Shared Parking Demand</u>	<u>Market Demand</u>	<u>Design Guidelines</u>
Office	67,600 sf	263	270	203
Retail	22,800 sf	83	114	92
Residential	41 units	<u>26</u>	<u>62</u>	<u>62</u>
		372	446	357

We have allocated 15 additional spaces for the residential uses even though they have garages. Construction in this block is not quite complete and field conditions may result in slight changes



Block Developments					
Block	Retail Space Sq. Ft.	Office Space Sq. Ft.	Residential No. of Units	Parking on Street No. of Spaces	Parking off Street No. of Spaces
2	75,200	64,452	N/A	64	344
3	60,900	55,000	4/A	82	466
4 (Open Market)	N/A	N/A	N/A	22	N/A
5	17,246	60,000	410u	45	325
6	42,000+16,891	6,000	55 du	56	244
8	23,380	N/A	119du	56	124
9 (Open Market)	N/A	N/A	N/A	26	N/A
10	22,200	N/A	192du	101	105
11	See 1100 notes	N/A	44du	69	108
Total	315,442	185,942	451du	521	1,756
					2,277

Illustrative Site Plan

New Town

JAMES CITY COUNTY, VIRGINIA

NEW TOWN

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June 22, 2004

By Hand

Ms. Karen Drake
Senior Planner
James City County, Virginia

RE: JCC Case No. C-07-03 New Town Shared Parking

Dear Karen:

As part of the submittal and approval process established by the JCC Planning Commission I am pleased to submit an analysis of the shared parking planned for Block 8 in New Town.

Included with the analysis is a chart showing parking allocations for Block 8, and also a larger chart that shows the location of Block 8 in New Town. Please note that the colored parking spaces are simply representations that parking is being appropriately allocated to various uses. This is not a representation that individual parking spaces will be assigned to these uses at these locations.

Please remember that although the uses in Block 8 are determined, the final sizes are not yet known since the buildings have not been designed. The number of apartment units may change very slightly, and the square footage of the retail/office building may change slightly as well. In addition, once construction of the parking area is underway there may slight variations as a result of locating handicap parking, dumpsters, or due to field conditions. New Town monitors this information and makes every effort to be sure that the finished product, both buildings and parking lots, provides appropriate parking as approved by James City County.

I am continuing to work on the parking analysis for the theater and Main Street retail uses. I have previously submitted parking plans for Blocks 2 and 5 and will attend the DRC meeting on July 7, 2004 and at that time I will be glad to provide updated information or answer questions regarding any parking issues in New Town. I will be unable to attend the DRC meeting on July 28 (my wife is having knee surgery that day) but someone else from New Town will attend to address any issues on Block 8.

Please let me know if you have additional requirements or would like additional information. Thank you.

Sincerely,
New Town Associates, LLC

Lawrence Salzman
Managing Director

**New Town
Block 8
Parking Report
June 2004**

Block 8 (bounded by New Town Avenue, Casey Boulevard and Center Street) is the next block planned for development in New Town. Block 8 will consist primarily of single family homes (both attached and detached) but will include some commercial uses at the corner of New Town Avenue and Center Street and along Center Street, and also a multi-family use along Center Street. The commercial uses, the multi-family use, and a few of the single family residences will be part of the shared parking plan in use at New Town

Using the shared parking concept, office uses are calculated at four spaces per 1,000 square feet of usable space, retail at 3.8 spaces per 1,000 square feet of floor area and residential uses at 1.5 spaces per unit. At the 2 P.M. peak hour office and retail uses are calculated at 97% of the above stated rates (based on Urban Land Institute studies) and residential uses at 55% of the rate. The overall parking calculation for the block would therefore be as follows:

<u>Block 8</u>				
<u>Use</u>	<u>Size (sf) or units</u>	<u>Parking Demand</u>	<u>2 PM Ratio</u>	<u>2 PM Demand</u>
Contract (retail)	10,000	38	.97	37
(office)	16,000*	58	.97	56
Contract (apts.)	40	60	.55	33
Contract (single-family)	8**	4	.55	<u>2</u>
Total				128

* Office is based on 90% usable efficiency for typical buildings

** Eight carriage houses are planned which will have one car garages, and each unit will also be allocated a one half shared parking space.

Total peak parking demand for Block 8 would therefore be 128 spaces. Parking supply is 118 spaces in the central lot, and approximately 17 spaces will be available on Center Street and New Town Avenue for a total of 135 spaces. There are 8 garages which are not counted in the supply because they won't be shared with other uses.

Office use in Block 8 will consist of about 14,400 square feet of usable space. This office space requires approximately 56 spaces at peak demand, and these spaces will primarily be used between 8:30 A.M. and 5:00 P.M. on weekdays. On evenings, weekends, and holidays these spaces will be essentially unoccupied. This block will also have about 10,000 feet of retail space. At the 2 P.M. peak parking time the retail use will require about 37 parking spaces. This block also includes 40 apartments which will require about 60 total parking spaces but will need only about 33 spaces at peak time. Eight carriage houses will back up to the central parking lot. Each of these homes will have one car garages. The 48 residential units (apartments and carriage houses) will share well with the office uses in this block.

Traditional market demand (4/1000 office, 5/1000 retail, 1.5 per unit) would total about 180 spaces. Design guidelines parking (3/1000 office, 4/1000 retail, 1.5 per unit) would indicate a maximum of about 155 spaces, and the block parking provided (including lined spaces but not garages) is 118 spaces. This indicates that about 62 fewer spaces are required than for free standing buildings without shared parking. Overall this block seems to demonstrate that a mix of uses, utilizing a shared central parking lot, garages and some on street parking, will all have adequate parking even at the times of their peak demand, and the overall community benefits by having less parking than if these were stand alone buildings.

Block 8 Summary

<u>Use</u>	<u>Size</u>	<u>Peak Shared Parking Demand</u>	<u>Market Parking</u>	<u>Design Guidelines</u>
Office	16,000 sf	56	58	43
Retail	10,000 sf	37	50	40
Residential	48 units	<u>35</u>	<u>72</u>	<u>72</u>
		128	180	155

This block will be across Center Street from the Village Green. Although peak parking is calculated to be at 2 P.M. on a weekday there may be evening or weekend activities at the Village Green which draw crowds. The offices in the area will generally be closed at these times which will free up significant amounts of parking to accommodate any crowds. However the apartments would be expected to have significant parking demands at the same time as these evening or weekend events. Other new urbanism projects have addressed this problem by assigning and reserving one parking space per apartment unit, and allowing the other one half space per unit to find parking where it is available. In Block 8 this will require 40 assigned and reserved spaces for the 40 apartment units (the carriage houses have garages) and will increase peak parking to a total of 135 spaces. Supply in the block and on Center Street and New Town Avenue is 135 spaces, and additional spaces are available on the west side of New Town Avenue just south of Center Street.

Overall the mix of uses in Block 8 is well suited for shared parking, and assigning and reserving one space per apartment unit should assure that parking is reasonably available to accommodate the apartment dwellers.

Construction in this block is planned but not yet underway, and no land sales in this block have closed yet. Slight changes due to field conditions, or slight changes when buildings are designed, may result in slight changes to the parking analysis for Block 8.

Block 8

Retail / Office	93
Apartments	40
Single Family	2
	<u>135</u>



New Town Ave.

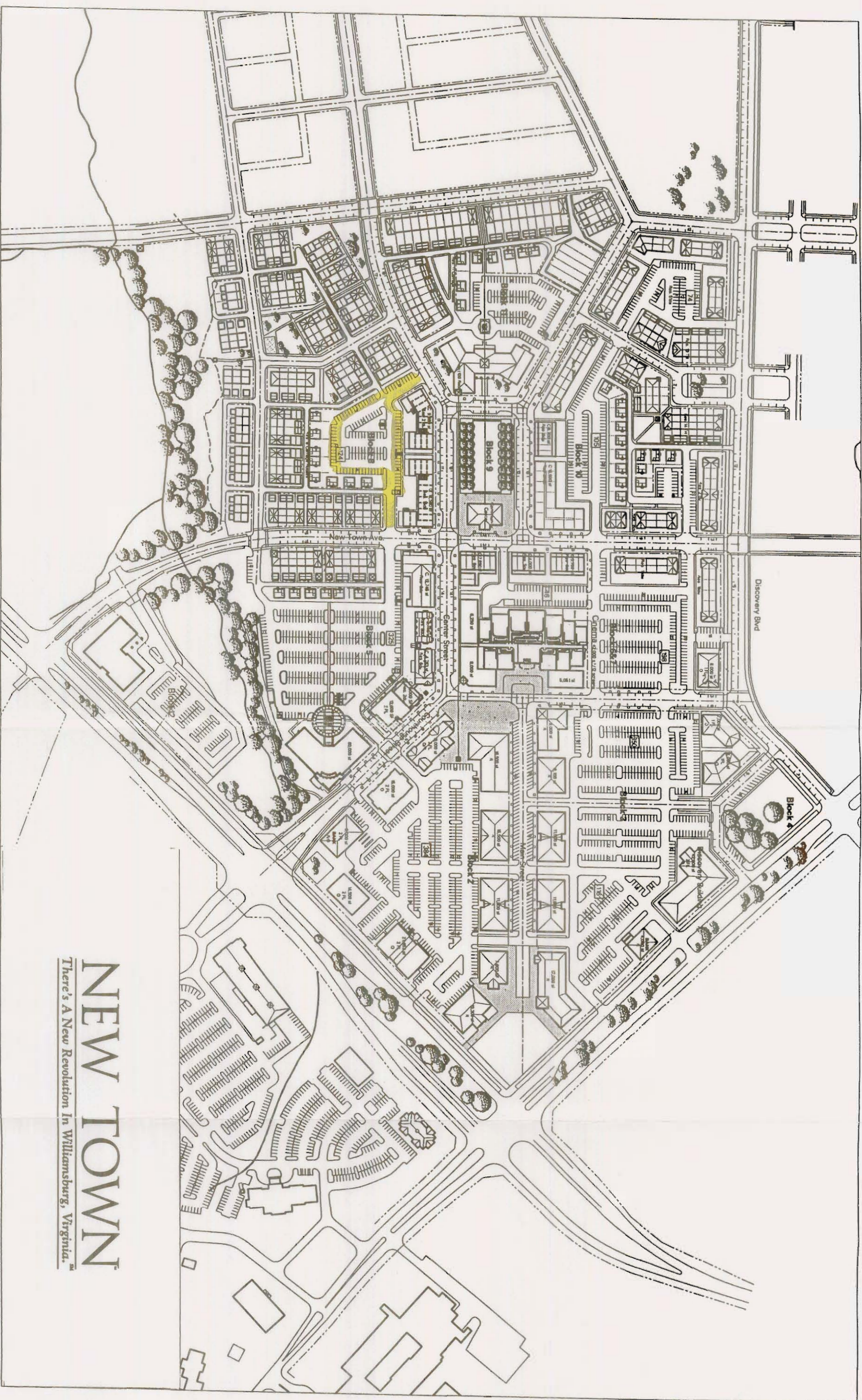
Apartments

Retail / Office

Block 8

Res. 8 Carriage Houses

Center Street



New Town Associates

Illustrative Site Plan

New Town
JAMES CITY COUNTY, VIRGINIA

NEW TOWN
There's A New Revolution In Williamsburg, Virginia.

Cooper, Robertson & Partners

March 25, 2004

SP-14-04

Action Park of Williamsburg (Go-Karts Plus) Ride Addition

Staff Report for the July 7, 2004 Development Review Committee Meeting

SUMMARY FACTS

Applicant: Mr. Bob Miller, Action Park of Williamsburg

Land Owner: Mr. Bob Miller

Location: 6870 Richmond Road

Tax Map/Parcel No: (24-3)(1-18)

Primary Service Area: Inside

Existing Zoning: B-1, General Business

Comprehensive Plan: Mixed Use

Overview: The plan proposes a new ride (DISK 'O' - schematic attached). The ride encompasses an area of approximately 2700 s.f. and is 32' at maximum height.

Reason for DRC review: The plans require DRC review because the park must abide by the conditions of its previously approved special use permit (SUP-34-94). A condition of the special use permit states that "Site plan approval by the Development Review Committee shall be required, including the submittal of a landscaping plan which protects adjacent properties and minimizes any adverse impacts on Richmond Road's function as a corridor within an historic area."

Staff Contact: Dave Anderson Phone: 253-6685

GENERAL DESCRIPTION

The ride will be located in an open field next to the Bumper Car Building. A parking lot is located between Richmond Road and the open field, so the proposed ride is located approximately 150' away from Richmond Road. The plan has undergone full site plan review by County agencies and meets all applicable requirements.

At the March 31, 2004 DRC meeting, the case was deferred to give the applicant the opportunity to develop a landscaping plan that helps screen the proposed ride from the Richmond Road corridor and from cars traveling in and out of the Colonial Heritage development. The applicant has since constructed a simulation of the ride by placing two 32' tall poles in the ground at either end of the proposed ride's extents. The applicant then strung a line of flags from the top of each pole, intersecting the ground at the center of the proposed ride and forming a V-shape, to simulate the track. Staff has viewed this simulation and believes it is a realistic approximation of the scale of the ride.

The applicant, with Staff's assistance to take photographs, then examined the visibility of the ride simulation traveling west on Richmond Road, east on Richmond Road, and from the main entrance of Colonial Heritage. As indicated in the attached photographs, the existing buffer does a good job of screening the majority of the simulation. The ride was not visible traveling west down Richmond Road, and was first visible traveling east down Richmond Road approximately at the location of the start of the right turn lane into Colonial

Heritage. At the beginning of the turn lane, the existing landscaping onsite largely screened the ride simulation, but towards the end of the turn lane the ride simulation was fairly visible through a break in the landscaping. The applicant agreed to plant an additional Bradford Pear tree, which are planted throughout the existing buffer, at this location. In the attached photographs, the proposed planting location is identified by the white flag. The applicant proposes to plant three additional Bradford Pears, the locations of which are identified by white flags on the attached photographs, to supplement the existing landscaping and screen the ride from Richmond Road and Colonial Heritage. In accordance with ordinance requirements, the trees must be a minimum of 2.5" caliper at the time of planting, equating to approximately 8-10' tall. Bradford Pears are fast growing trees, growing 12'-15' within 8 to 10 years. The additional plantings, and their specific locations, will be made part of site plan approval. Prior to placement of the proposed ride, staff will ensure that the plantings have been installed in accordance with the specified locations and at the ordinance required caliper. In general, staff prefers that Bradford Pears trees not be used due to their susceptibility to storm damage. However, staff is willing to make an exception in this case due to the large number of Bradford Pear trees already on the site. Should these trees be extensively damaged in the future, their replacement will be required in order to meet the conditions of the SUP.

The ride does have lights which will be turned on at night. However, due to the minimum expected visibility of the ride and the relative brightness of the existing site lighting, staff does not believe the additional lighting will have an appreciable negative effect on either Richmond Road or adjacent properties, including Colonial Heritage.

STAFF RECOMMENDATION

Upon further review of the visibility of the proposed ride from the Richmond Road corridor and Colonial Heritage, staff believes the existing landscaping supplemented by the additional Bradford Pear trees will adequately screen the ride and protect the character of Richmond Road as a community character corridor. Staff recommends the Development Review Committee approve SP-14-04, Action Park of Williamsburg Ride Addition.

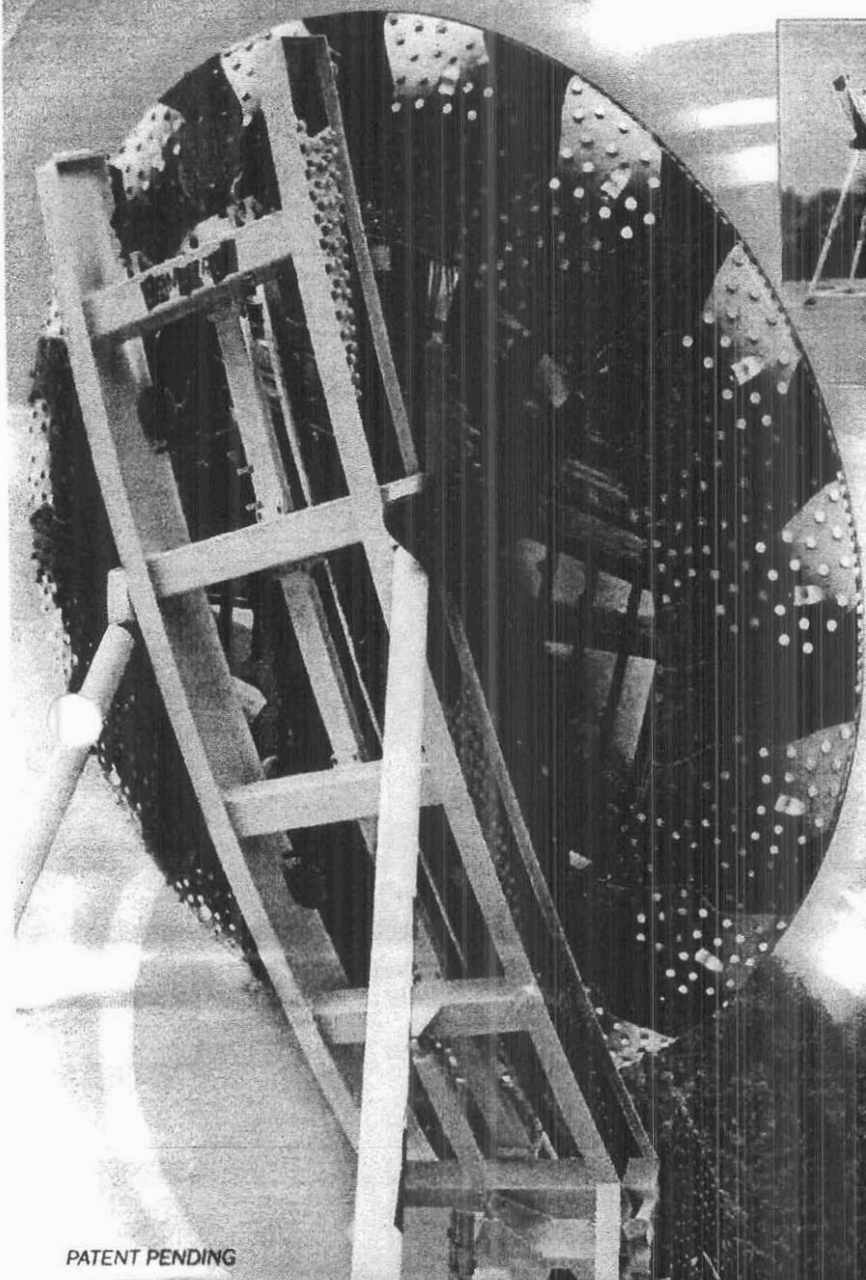

Dave Anderson

Attachments:

1. Ride Schematic
2. Ride Simulation Photographs
3. Site Plan

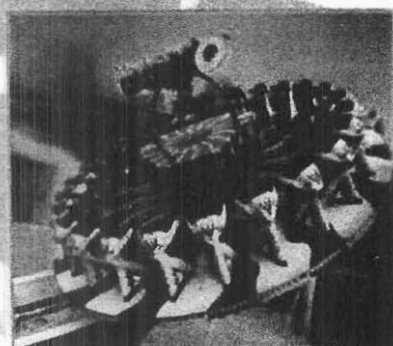
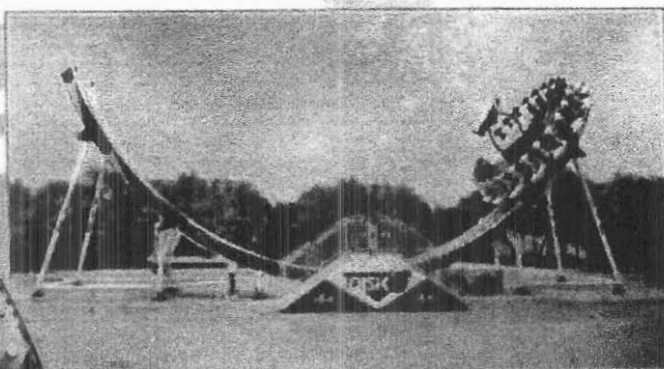
A M U S E M E N T R I D E S

A N U F A C T U R E R S

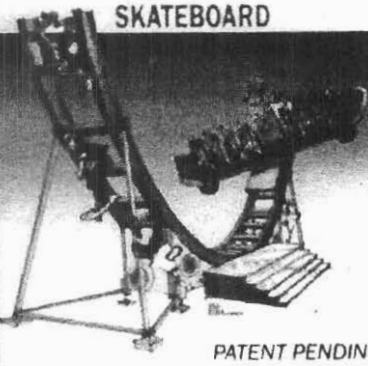


PATENT PENDING

DISK'O'
New thrill ride 2002



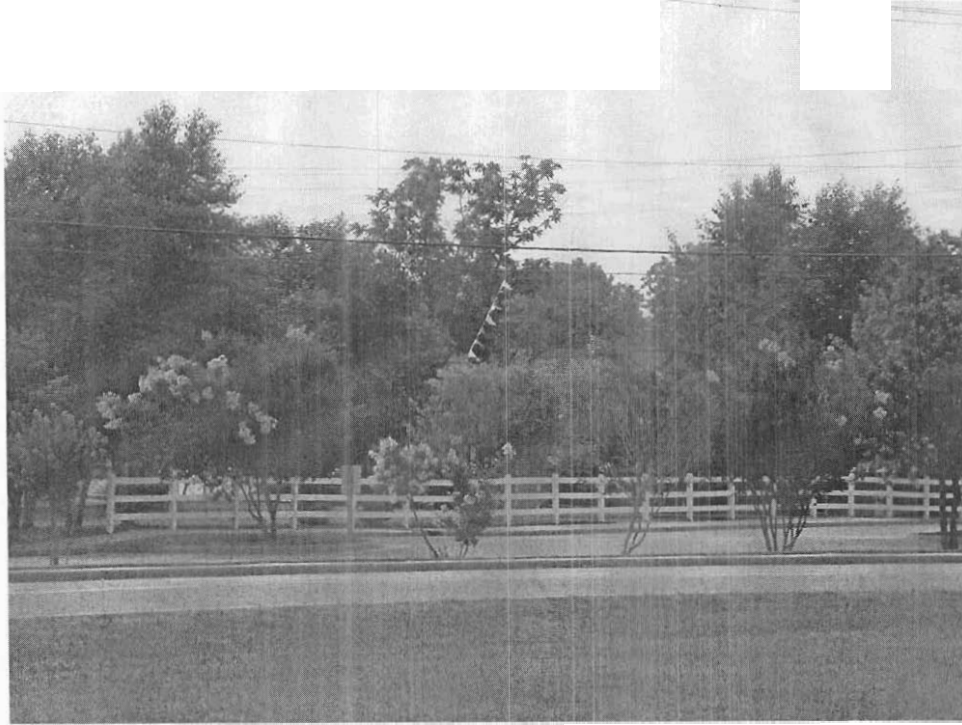
SKATEBOARD



PATENT PENDING

Driving West on Richmond Road Towards Go-Karts Plus:

1st point at which you can see the ride (approximately at the beginning of the right turn lane into Colonial Heritage) – top of ride somewhat visible but largely blocked by trees



Further down the road towards Colonial Heritage – top of ride a little less visible and still largely blocked by trees.



Close to the entrance into Colonial Heritage – Ride somewhat visible. One tree proposed in the location identified by the white flag.



Pulling out of Colonial Heritage's main entrance (going west) – Ride fairly visible. Two trees proposed in the locations identified by the white flags.



Pulling out of Colonial Heritage's main entrance (going east and looking back) – Ride fairly visible. One tree proposed in the location identified by the white flag.



Traveling east on Richmond Road further down from Colonial Heritage's main entrance (looking back) – Very tip of the ride barely visible.



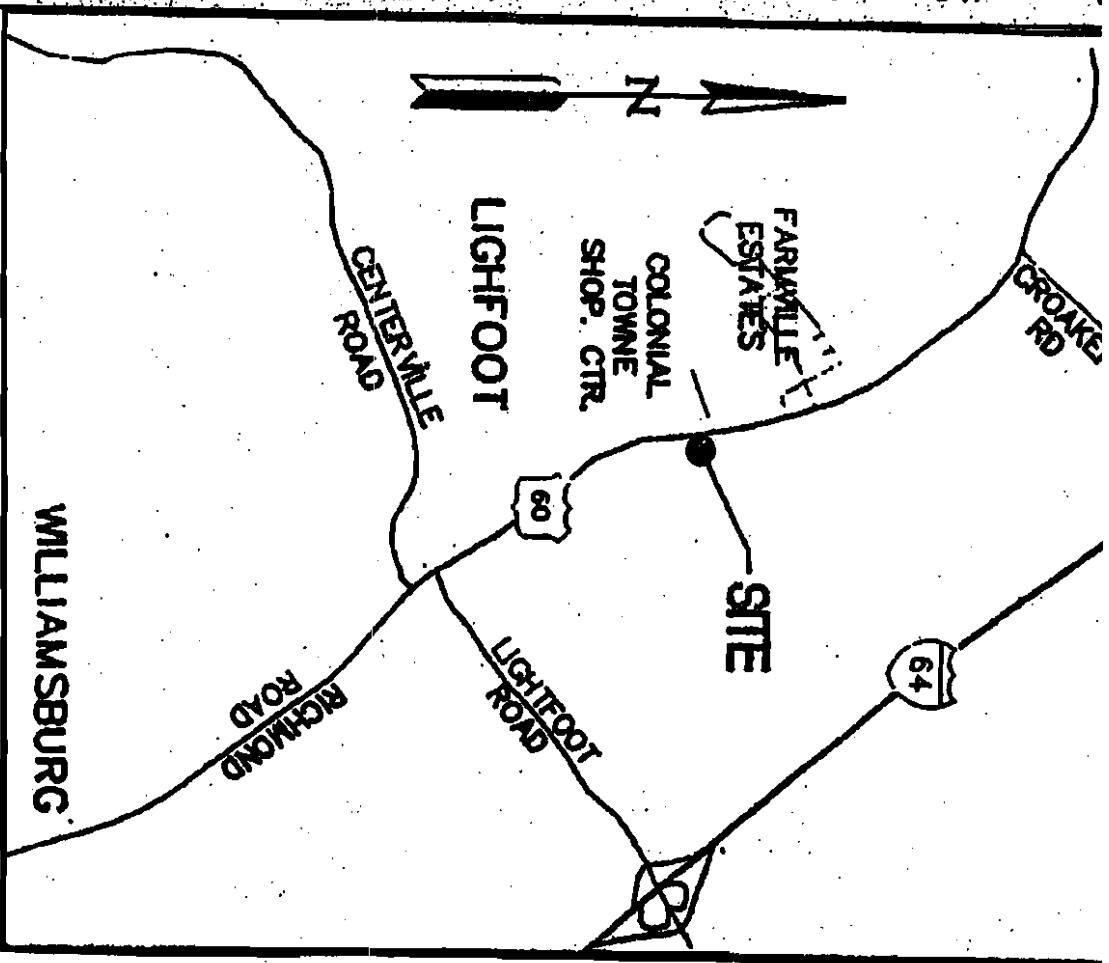
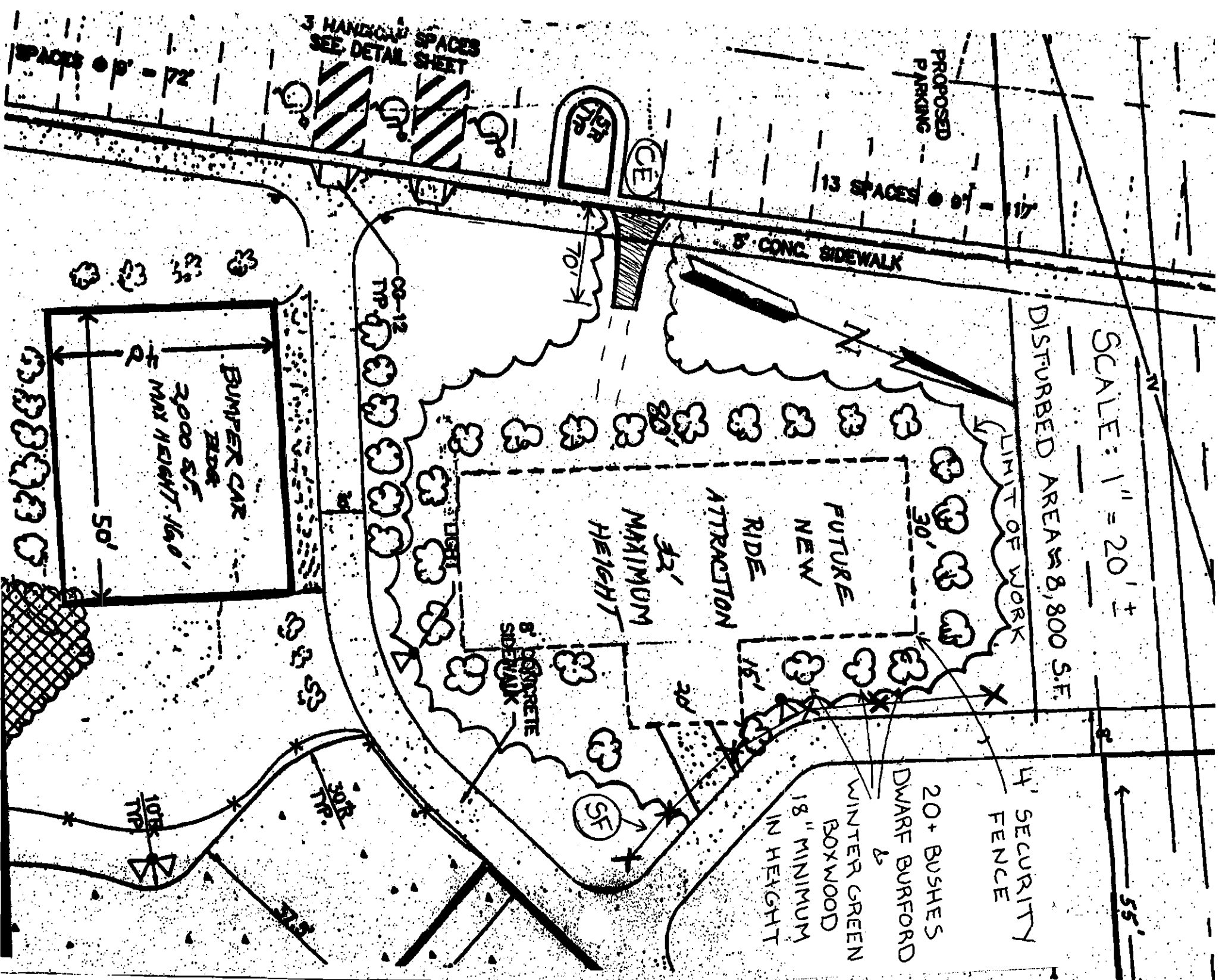
Driving East on Richmond Road Towards Go-Karts Plus:

Coming around the corner – Proposed ride not visible.



Adjacent to the proposed ride location – Proposed ride not visible.





VINICITY MAP
SCALE: 1"=200'

SP-14-04: Action Park of Williamsburg (Go-Karts Plus) Ride Addition

- Site is located at 6870 Richmond Road
- Property is zoned B-1, General Business
- 3.82 acres (total park - 8.094)
- Tax Map ID (24-3)(1-18)
- Stonehouse Magisterial District

General Notes:

- Amendment to SP-10-04 to add a new amusement attraction encompassing an area of approximately 2700 sq. ft. with a maximum height of 32'.
- Evergreen shrubs shall be a minimum of 18" in size at the time of planting.
- No new utilities are proposed.
- See SUP-34-94 for conditions.



COUNTY OF JAMES CITY FINAL SITE PLAN	
APPROVALS	DATE
Fire Dept. _____	_____
Health Dept. _____	_____
VDOT _____	_____
Planning _____	_____
Environ _____	_____
Zoning _____	_____
JOSA _____	_____
County Eng _____	_____
REA _____	_____
Other _____	_____

**Case No. SP-59-04. Norge Neighborhood Site Plan
Staff Report for July 7, 2004, Development Review Committee Meeting**

SUMMARY FACTS

Applicant: Jason Grimes

Landowner: Norge Neighborhood LLC
John E. Dodson of Williamsburg Dodge

Proposal: Construct 80 multi-family units in 20 buildings

Location: 7101, 7145 and 7147 Richmond Road, 126 Rondane Place
75 Nina Lane

Tax Map/Parcel No.: (23-2) (1-50), (1-50C), (1-49), (1-51) and (24-1)(1-8)

Primary Service Area: Inside

Parcel Size: 21.03 acres (total)

Existing Zoning: MU with proffers and B-1

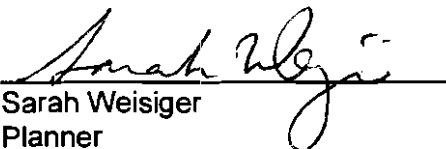
Comprehensive Plan: Low Density Residential

**Reason
for DRC Review:** Multi-family unit development of more than 50 units.

Staff Contact: Sarah Weisiger, Planner **Phone:** 253-6685

STAFF RECOMMENDATION:

The applicant has requested that this case be deferred until the next DRC meeting, July 28, 2004. Prior to the next meeting, the applicant expects to have submitted a revised site plan addressing the Environmental Division's agency comments which had prevented recommendation for preliminary approval of the case at the June 2, 2004 DRC meeting.


Sarah Weisiger
Planner

Case No. SP-51-04. Druid Hills Section D
Staff Report for July 7, 2004, Development Review Committee Meeting

SUMMARY FACTS

Applicant: Kenneth Jenkins of LandTech Resources, Inc.

Landowner: Andy Piplico of C D & A, Inc.

Proposal: To construct a road within an existing right-of-way.

Location: Braddock Court between and behind 112 Braddock Road and 1 Braddock Court (also known as 110 Braddock Road).

Tax Map/Parcel Nos.: Road will provide access to Parcel Nos. (7-82) which has an existing residence, and (7-83), (7-84), (7-85) and (7-86) on JCC Tax Map (47-2)

Primary Service Area: Inside

Site Area: 2.337 acres

Existing Zoning: R-1, Limited Residential

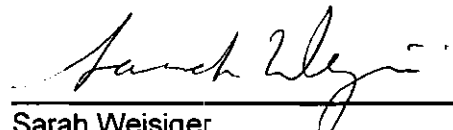
Comprehensive Plan: Low Density Residential

Reason for DRC Review: Unresolved problems between the applicant and adjacent property owners. Section 24-147 (a)(2)

Staff Contact: Sarah Weisiger, Planner Phone: 253-6685

STAFF RECOMMENDATION:

At its meeting on June 2, 2004, the DRC deferred action on the case to allow time for neighbors who had specific concerns related to the development to provide documentation substantiating their concerns. Staff has made case documents and emails available to interested citizens in order to comply with a Freedom of Information Act request by an adjacent property owner. One of the residents has requested clarification of staff correspondence on the case, but no other documents have been provided by the neighbors to staff. With regard to road design, staff notes that roll top curbs have been provided along the cul-de-sac portion of the road. The design will reduce clearing and grading necessary for road construction without increasing pavement area; staff supports this change. Finally, staff continues to recommend preliminary approval of the site plan subject to agency comments.



Sarah Weisiger
Planner

Attachments:
Agency comments
Site plan (under separate cover)

Agency Review Comments
for
SP- 51-04 Druid Hills Section D

Planning:

1. No comments.

County Engineer:

1. No comments.

Fire:

1. No comments.

Virginia Department of Transportation:

1. See attached memorandum dated June 7, 2004.

Environmental:

1. See attached memorandum dated June 30, 2004.

JCSA:

1. Comments will be provided as soon as they are made available.



COMMONWEALTH of VIRGINIA

DEPARTMENT OF TRANSPORTATION
4451 IRONBOUND ROAD
WILLIAMSBURG, VA 23188



STEVEN W. HICKS
RESIDENT ENGINEER
TEL (757) 253-4832
FAX (757) 253-5148

PHILIP SHUCET
COMMISSIONER

June 7, 2004

Sarah Weisiger
James City County Planning
Post Office Box 8784
Williamsburg, Virginia 23187

Ref: Druid Hills Section D (Re-subdivision)
SP-51-04
Braddock Road (Route 732), James City County

Dear Ms. Weisiger:

We have completed our review of the above mentioned development plan and offer the following comments:

- 1) Provide inlet computations for 15" RCP (inlet of the storm sewer system).
- 2) Please review the Coefficient "C" factors of the Storm Drainage Design.
- 3) A minimum of 2' cover is required for the RCP under Braddock Court.

When the above comments have been addressed, please submit two sets of revised plans to this office for further review. Also, attach a letter noting what action was taken to correct the above comments and any revisions that may impact the right-of-way.

Should you have any questions please contact me at 253-4832.

Sincerely,

Anthony L. Handy, PE, LS
Assistant Resident Engineer

James City County Environmental Division
Resubdivision of a portion of Druid Hills, Section D
SP-51-04 (replaces S-07-04)
June 30, 2004

WAC/DC



General Comments:

1. Guard Rail. Due to the excessive slopes proposed at the entrance to the cul-de-sac and the potential threat to public safety it may pose, it is strongly recommend that some type of barrier or guard rail be proposed on the south side of Braddock Court between stations 10+25 and 11+00. It is not often that a new comment arises at this stage of plan review, and we apologize for any inconvenience it may impose, but due to the importance of the issue, it cannot be overlooked.
2. Wetlands Permit. Provide evidence that any necessary wetlands permits have been obtained or are not necessary for this project.

Erosion & Sediment Control Plan:

3. Drainage Calculations. Provide design information for the new drainage system installed on lot 83.

DEVELOPMENT REVIEW COMMITTEE ACTION REPORT
Meeting of July 7, 2004

Case No. S-037-04 / SP-056-04

Michelle Point

Mr. Jay Epstein of Michelle Point LLC submitted subdivision and site plans proposing the creation of single-family and townhouse units in Michelle Point. The property is located at 9001 Barhamsville Road and is further identified as parcel (1-3) on James City County Tax Map (12-1). DRC review is necessary for the following reasons: That the proffers for this development state that there shall be a variable width, undisturbed buffer along Route 30 frontage subject to approval by the DRC; that the proffers also state that the owner shall provide recreational facilities subject to the approval of the DRC; and that the project proposes more than fifty residential units.

DRC Action: The DRC approved the recreation facilities and the location of certain features in the buffer along Route 30. The DRC also recommended preliminary approval be issued subject to agency comments and Chesapeake Bay Board requirements.

Case No. SP-072-04

ECC Building

James City County Fire Chief Richard Miller has submitted a site plan proposing a 7,156 square foot emergency communications center. The site is located at 3127 Forge Road and is further identified as parcel (1-27) on James City County Tax Map (13-3). DRC review is necessary for any public area or facility use not shown on the Comprehensive Plan.

DRC Action: The DRC found the application consistent with the Comprehensive Plan.

Case No. SP-080-04

EOC Tower

James City County Fire Chief Richard Miller has submitted a site plan proposing a 160-foot communications tower to serve as part of the JCC 800-MHz trunked radio system. The site is located at 3127 Forge Road and is further identified as parcel (1-27) on James City County Tax Map (13-3). DRC review is necessary for any public area or facility use not shown on the Comprehensive Plan.

DRC Action: The DRC found the application consistent with the Comprehensive Plan.

Case No. SP-069-04

New Town – Block 5, Parcels D+E

Mr. Bob Cosby of AES Consulting Engineers submitted a site plan proposing the approval of four mixed use buildings: Building A-1 shares a common wall with the Corner Pocket and will contain 3,855 square feet of retail space and two residential units; Building A contains 4,381 square feet of retail and contains six residential units; Building B contains 4,038 square feet of retail space and eight residential units; and Building C contains 7,336 square feet of retail, 7,183 of commercial square footage and six residential units. The parcels are further identified as parcel (24-7) on James City County Tax Map (38-4) and parcel (24-1) on Tax Map (38-2). DRC review is required as the proposed buildings exceed 30,000 of total floor area.

DRC Action: The DRC unanimously recommended preliminary approval subject to agency comments.

Case No. C-007-03**Town Center Parking Overview**

Mr. Bob Cosby of AES Consulting Engineers, on behalf of Mr. Bob Ripley of GCR, submitted a conceptual plan for parking for Blocks 2, 5, and 8 in New Town. The property is along Monticello Avenue and is further identified as parcel (1-50) on James City County Tax Map (38-4). DRC review is required because the application proposes general off-site parking and shared parking for the aforementioned blocks and also proposes automatic review for off-site and shared parking for New Town.

DRC Action: the DRC voted unanimously to approve the July, 2004 quarterly update for New Town Section 2&4, Blocks 2, 5 & 8 shared parking and off-site parking with the October 2004 quarterly update to be placed on the DRC consent agenda.

Case No. SP-014-04**Go-Karts Plus Ride**

Mr. Bob Miller of Action Park of Williamsburg submitted a site plan amendment proposing a new ride at Action Park. The ride encompasses an area of approximately 2700 s.f. and is thirty-two feet high at maximum height. The proposed site is located at 6780 Richmond Road, and is further identified as parcel (1-18) on James City County Tax Map (24-3). The park must abide by the conditions of SUP-34-94, which require DRC review of site plan approval and any amendments.

DRC Action: The DRC recommended denial of the plan by a vote of 3-1.

Case No. SP-59-04**Norge Neighborhood**

Mr. Jason Grimes of AES Consulting Engineers, on behalf of Norge Neighborhood LLC, submitted a site plan proposing 80 multi-family units to be located on 7101, 7145, and 7147 Richmond Road, 126 Rondane Place, and 75 Nina Lane. The parcels are further identified, respectively, as parcels (1-50), (1-50C), (1-49), and (1-51) on James City County Tax Map (23-2) and parcel (1-8) on Tax Map (24-1). DRC review is necessary for any site plan proposing fifty or more residential units.

DRC Action: The DRC deferred action on the case.

Case No. SP-51-04**Druid Hills, Section D**

Mr. Kenneth Jenkins of LandTech Resources, on behalf of Andy Piplico of CD&A, submitted a site plan proposing the construction of a road within existing right-of-way in the Druid Hills subdivision. The road will provide access to parcels (7-83), (7-84), (7-85), and (7-86) on James City County Tax Map (47-2). DRC review is necessary due to unresolved problems between the applicant and adjacent property owners.

DRC Action: The DRC recommended preliminary approval by unanimous voice vote.

**JAMES CITY COUNTY
DEVELOPMENT REVIEW COMMITTEE REPORT**
FROM: 6/1/2004 THROUGH: 6/30/2004

I. SITE PLANS

A. PENDING PRELIMINARY APPROVAL

SP-087-01	The Vineyards, Ph. 3
SP-112-02	Ford's Colony Recreation Park
SP-035-03	Prime Outlets, Ph. 5-A & 5-B - SP Amend.
SP-045-03	Noah's Ark Vet Hospital SP Amend.
SP-052-03	Kingsmill Access Ramp for Pool Access Bldg.
SP-063-03	District Park Sports Complex Parking Lot Expansion
SP-079-03	Tequila Rose Walk-in Cooler
SP-086-03	Colonial Heritage Golf Course
SP-095-03	KTR Stonemart
SP-131-03	Colonial Heritage Ph. 2, Sec. 1
SP-132-03	Windy Hill Market Gas Pumps & Canopy SP Amend.
SP-145-03	Williamsburg National 13 Course Expansion
SP-006-04	Williamsburg Christian Retreat Center Amend.
SP-014-04	Action Park of Williamsburg Ride
SP-016-04	Richardson Office & Warehouse
SP-025-04	Carter's Cove Campground
SP-041-04	Ford's Colony - Country Club Redevelopment SP Amd.
SP-047-04	Villages at Westminster Drainage Improvements
SP-050-04	AJC Woodworks
SP-051-04	Druid Hills, Sec. D
SP-054-04	Milanville Kennels
SP-056-04	Michelle Point
SP-059-04	Norge Neighborhood
SP-063-04	Merrimac Center Project Greenhouse
SP-067-04	Treyburn Drive Courtesy Review
SP-069-04	New Town - Block 5, Parcel D & E, Mixed Use Bldgs.
SP-070-04	Godspeed Animal Care
SP-072-04	ECC Building
SP-074-04	Chesapeake Bank
SP-076-04	Stonehouse Recreational Vehicle Storage Area
SP-077-04	George Nice Adjacent Lot SP Amend.
SP-078-04	First Advantage Federal Credit Union
SP-079-04	Norge Railway Station
SP-080-04	JCC Communications Tower - EOC

B. PENDING FINAL APPROVAL

SP-050-03	Wmbg-Jamestown Airport T-Hanger & Parking Exp.
SP-056-03	Shell Building - James River Commerce Center

EXPIRE DATE

7/29/2004
3/ 4/2005

Wednesday, June 30, 2004

Page 1 of 4

SP-091-03	Colonial Heritage Ph. 1, Sec. 5	8/ 4/2004
SP-092-03	Ford's Colony - Westbury Park, Recreation Area #2	9/ 8/2004
SP-108-03	Fieldstone Parkway Extension	2/26/2005
SP-116-03	Kingsmill - Armistead Point	11/19/2004
SP-134-03	Ironbound Center 4	12/15/2004
SP-136-03	GreenMount Industrial Park Road Extension	3/15/2005
SP-138-03	New Town - Prudential-McCardle Office Building	12/29/2004
SP-140-03	Pocahontas Square	3/ 1/2005
SP-141-03	Colonial Heritage - Ph. 2, Sec. 3	1/12/2005
SP-143-03	New Town - United Methodist Church	1/12/2005
SP-150-03	WindsorMeade Marketplace	2/ 3/2005
SP-003-04	WindsorMeade Villas	3/ 1/2005
SP-004-04	WindsorMeade - Windsor Hall	3/ 1/2005
SP-005-04	WindsorMeade - Villa Entrance & Sewer Const.	3/ 3/2005
SP-015-04	New Town - Sec. 4, Ph. 2 Infrastructure	4/ 5/2005
SP-017-04	Settlement at Monticello - Community Club	4/ 6/2005
SP-018-04	New Town - Block 8, Ph. 1B	6/ 7/2005
SP-023-04	Williamsburg Landing SP Amend.	4/ 2/2005
SP-027-04	Greensprings Condominiums SP Amend.	6/ 7/2005
SP-045-04	Powhatan Co-Location Monopole Tower	4/29/2005
SP-057-04	The Archaearium at Historic Jamestowne	6/15/2005
SP-060-04	New York Deli	6/10/2005
SP-064-04	Eckerd's at Powhatan Secondary	6/17/2005

C. FINAL APPROVAL

DATE

SP-009-03	Energy Services Group Metal Fabrication Shop	6/10/2004
SP-127-03	New Town - Old Point National Bank	6/16/2004
SP-139-03	New Town - Block 8, Ph. 1	6/15/2004
SP-009-04	Colonial Heritage - Ph. 1, Sec. 3 & 3A	6/11/2004
SP-013-04	Gabriel Archer - Williamsburg Winery - SP Amend.	6/18/2004
SP-028-04	Shiloh Baptist of Croaker	6/ 7/2004
SP-034-04	Lafayette H.S. Trailer Addition	6/ 2/2004
SP-037-04	Stonehouse ES Trailer Addition	6/ 2/2004
SP-042-04	Dream Catchers Therapeutic Riding Center	6/21/2004
SP-046-04	Williamsburg Cancer Treatment Center SP Amend.	6/29/2004
SP-058-04	Dominion Power - Maintenance Building	6/ 8/2004
SP-065-04	Jamestown High School PTSA Sign	6/15/2004
SP-066-04	Jamestown High School Modular Storage Bldg.	6/ 2/2004
SP-068-04	Owens-Brockway Glass Container - Concrete Slab	6/ 8/2004
SP-071-04	Kingsmill Resort Tent	6/28/2004
SP-073-04	Jamestown Island - Starving Times Café	6/28/2004
SP-075-04	Nicewood	6/17/2004

D. EXPIRED

EXPIRE DATE

II. SUBDIVISION PLANS

A. PENDING PRELIMINARY APPROVAL

S-104-98	Skiffes Creek Indus. Park, VA Trusses, Lots 1,2,4
S-013-99	JCSA Mission Bank ROW Acquisition
S-074-99	Longhill Station, Sec. 2B
S-110-99	George White & City of Newport News BLA
S-091-00	Greensprings West, Plat of Subdv Parcel A&B
S-032-01	Subdivision and BLE Plat of New Town AssociatesLLC
S-008-02	James F. & Celia Ann Cowles Subdivision
S-086-02	The Vineyards, Ph. 3, Lots 1, 5-9, 52 BLA
S-062-03	Hicks Island - Hazelwood Subdivision
S-066-03	Stonehouse, BLA & BLE Parcel B1 and Lot 1, Sec. 1A
S-067-03	Ford's Colony Sec. 33, Lots 1-49
S-083-03	Columbia Drive Subdivision
S-094-03	Brandon Woods Parkway ROW
S-100-03	Colonial Heritage Ph. 2, Sec. 1
S-101-03	Ford's Colony - Sec. 35
S-107-03	Stonehouse Conservation Easement Extinguishment
S-108-03	Leighton-Herrmann Family Subdivision
S-116-03	Stonehouse Glen, Sec. 2
S-003-04	Monticello Ave. ROW plat for VDOT
S-008-04	Lake Powell Forest Ph. 6
S-021-04	Varble Subdivision
S-022-04	ROW Conveyence for Rt. 5000 & Rt. 776 Abandonment
S-027-04	Lake Powell Forest Ph. 7
S-029-04	BLA Lots 1A & 1B Longhill Gate
S-034-04	Warhill Tract BLE / Subdivision
S-037-04	Michelle Point
S-046-04	ARGO Ph. 2
S-047-04	ARGO Ph. 3
S-048-04	Colonial Heritage - Open Space Easement
S-054-04	6096 Centerville Road Subdivision
S-055-04	117 Winston Terrace
S-056-04	603 and 604 Dogleg BLA
S-057-04	Boughsprings Resubdivision of Lot 22B
S-058-04	New Town - Block 2, Parcel D
S-059-04	Greensprings West Ph. 6
S-060-04	Williamsburg Jamestown Airport

B. PENDING FINAL APPROVAL

		EXPIRE DATE
S-037-02	The Vineyards, Ph. 3	5/ 4/2005
S-076-02	Marion Taylor Subdivision	10/ 3/2004
S-094-02	Powhatan Secondary Ph. 7-C	12/30/2004
S-108-02	Scott's Pond, Sec. 3	1/13/2005

Wednesday, June 30, 2004

Page 3 of 4

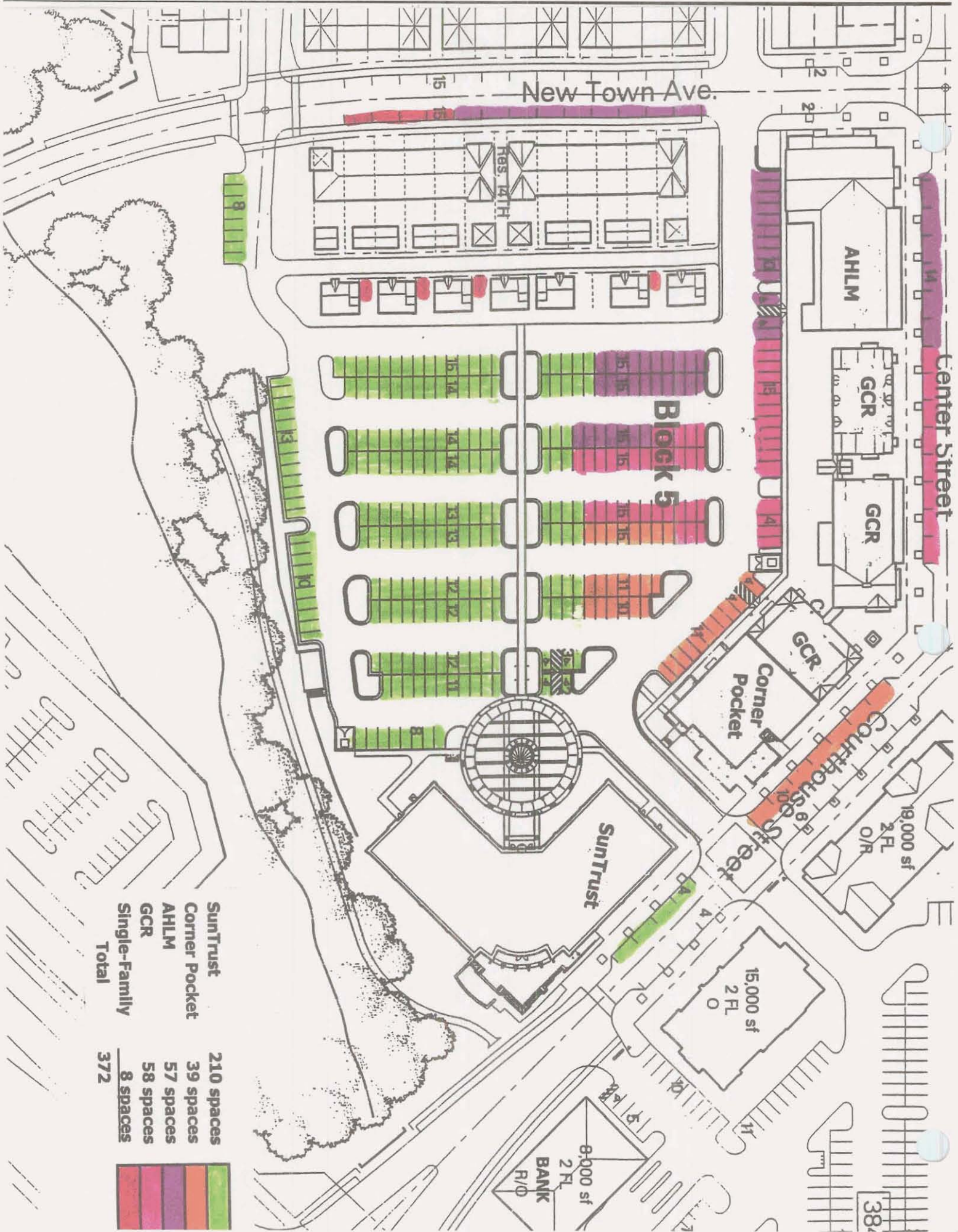
S-033-03	Fenwick Hills, Sec. 2	10/31/2004
S-044-03	Fenwick Hills, Sec. 3	6/25/2005
S-049-03	Peleg's Point, Sec. 5	7/ 3/2004
S-055-03	Colonial Heritage Ph. 1, Sec. 5	8/ 4/2004
S-056-03	Colonial Heritage Ph. 1, Sec. 4	9/23/2004
S-057-03	Ford's Colony - Sec. 34	8/19/2004
S-073-03	Colonial Heritage Ph. 2, Sec. 2	10/ 6/2004
S-076-03	Wellington, Sec. 4	11/ 3/2004
S-078-03	Monticello Woods - Ph. 2	11/ 3/2004
S-098-03	Stonehouse Glen, Sec. 1	4/ 5/2005
S-099-03	Wellington, Sec. 5	2/ 3/2005
S-106-03	Colonial Heritage Ph. 2, Sec. 3	1/12/2005
S-001-04	Ironbound Village Ph. 2, Parcel 2	2/17/2005
S-002-04	The Settlement at Monticello (Hiden)	3/ 1/2005
S-007-04	Druid Hills, Sec. D Resubdivision	3/12/2005
S-009-04	Colonial Heritage Public Use Site B	3/18/2005
S-033-04	2011 Bush Neck Subdivision	5/ 4/2005
S-035-04	Colonial Heritage Blvd. Ph. 2 Plat	4/28/2005
S-036-04	Subdivision at 4 Foxcroft Road	6/15/2005
S-038-04	Greensprings West Ph. 4B & 5	6/ 9/2005
S-039-04	Governor's Land - Wingfield Lake Lots 27, 28	6/14/2005
S-041-04	6199 Richmond Road Subdivision	6/14/2005
S-042-04	Eckerd's at Powhatan Secondary	6/17/2005
S-044-04	8715 Pocahontas Trail BLE	5/20/2005
S-045-04	ARGO Ph. 1	6/28/2005
S-049-04	Norge Neighborhood	6/18/2005
S-050-04	Colonial Heritage - Golf Maintenance ROW	6/15/2005
S-051-04	WindsorMeade Marketplace	6/17/2005
S-052-04	The Villages at Powhatan, Ph. 7	6/15/2005
S-053-04	The Colonial Heritage Club	6/21/2005

C. FINAL APPROVAL

		DATE
S-058-03	Ford's Colony - Sec. 10, 171-172	6/16/2004
S-115-03	Eagle Tree Farm Lot 12	6/23/2004
S-006-04	Colonial Heritage - Ph. 1, Sec. 3 & 3A	6/11/2004
S-013-04	Wexford Hills Ph. 2	6/ 1/2004
S-017-04	Green Mount Lot 1A	6/14/2004
S-024-04	161 Old Stage Road Subdivision	6/30/2004

D. EXPIRED

EXPIRE DATE



SunTrust	210 spaces
Corner Pocket	39 spaces
AHLM	57 spaces
GCR	58 spaces
Single-Family	8 spaces
Total	372



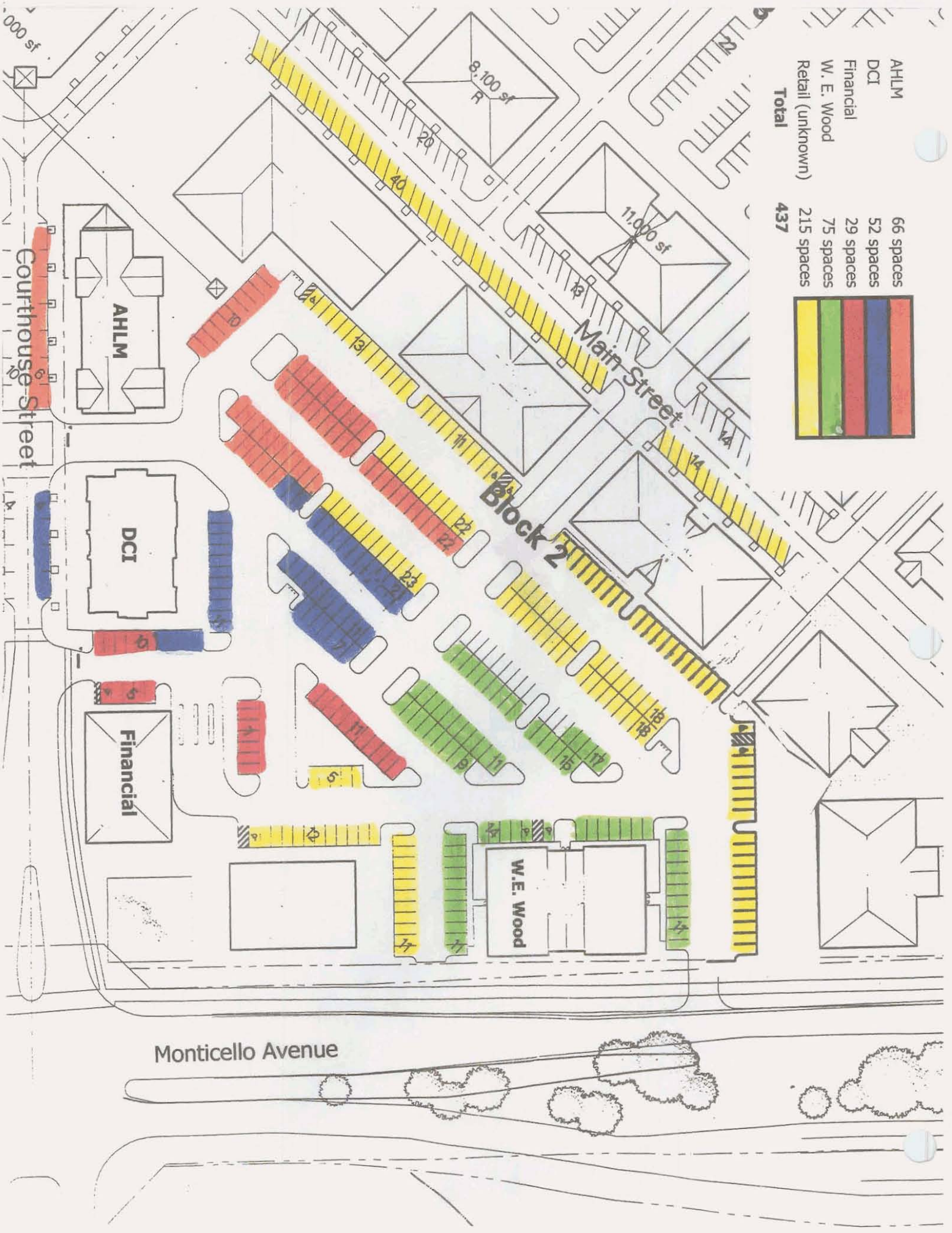
AHLM
DCI
Financial
W. E. Wood
Retail (unknown)

66 spaces
52 spaces
29 spaces
75 spaces
215 spaces



Total

437



AGENDA

DEVELOPMENT REVIEW COMMITTEE

July 7, 2004

4:00 p.m.

JAMES CITY COUNTY GOVERNMENT COMPLEX

Conference Room, Building C

1. Roll Call
2. Minutes
 - A. Meeting of June 2, 2004
3. Cases
 - A. S-37-04/SP-56-04 Michelle Point
 - B. SP-72-04 ECC Building
 - C. SP-80-04 EOC Tower
 - D. SP-68-04 New Town – Block 5 Parcels D & E
 - E. C-7-03 New Town Parking
 - F. SP-14-04 Go-Karts Plus Ride
 - G. SP-59-04 Norge Neighborhood
 - H. S-51-04 Druid Hills – Braddock Court
4. Adjournment