

A G E N D A
JAMES CITY COUNTY DEVELOPMENT REVIEW COMMITTEE
REGULAR MEETING
Building A Large Conference Room
101 Mounts Bay Road, Williamsburg, VA 23185
August 24, 2022
4:00 PM

A. CALL TO ORDER

B. ROLL CALL

C. MINUTES

1. April 20, 2022 Meeting Minutes

D. OLD BUSINESS

E. NEW BUSINESS

1. SP-22-0005. Colonial Veterinary Clinic Parking Expansion

F. ADJOURNMENT

ITEM SUMMARY

DATE: 8/24/2022
TO: The Development Review Committee
FROM: Paul D. Holt, III, Secretary
SUBJECT: April 20, 2022 Meeting Minutes

ATTACHMENTS:

	Description	Type
☐	Minutes of the April 20, 2022 DRC Meeting	Minutes

REVIEWERS:

Department	Reviewer	Action	Date
Development Review Committee	Crump, Josh	Approved	8/17/2022 - 10:50 AM
Development Review Committee	Secretary, DRC	Approved	8/17/2022 - 2:40 PM

MINUTES
JAMES CITY COUNTY DEVELOPMENT REVIEW COMMITTEE
REGULAR MEETING
Building A Large Conference Room
101 Mounts Bay Road, Williamsburg, VA 23185
April 20, 2022
4:00 PM

A. CALL TO ORDER

Ms. Barbara Null called the Development Review Committee (DRC) meeting to order at 4 p.m.

B. ROLL CALL

Present:

Barbara Null, Chair
Frank Polster
Stephen Rodgers
Rob Rose

Also in Attendance:

Jack Haldeman

Staff in Attendance:

Josh Crump, Principal Planner
John Risinger, Planner
Thomas Wysong, Senior Planner
Katie Pelletier, Community Development Assistant

C. MINUTES

1. March 30, 2022 Meeting Minutes

Mr. Polster motioned to Approve the March 30, 2022, DRC meeting minutes.

On a voice vote, the Motion passed 4-0.

D. OLD BUSINESS

There was no old business.

E. NEW BUSINESS

1. C-22-0019. 7607 Richmond Road - Oakland Farm Residential Rezoning

Mr. Risinger greeted the Committee and stated that Mr. John Grier from Plus Management Commercial Real Estate has submitted a conceptual plan for a proposed residential development located at 7607 Richmond Road. He said the parcel is currently zoned A-1, General Agriculture and is designated Low Density Residential (LDR) on the 2045 Comprehensive Plan Land Use map. Mr. Risinger said the parcel is inside the Primary Service Area and is approximately 95 acres in size. He noted the proposed development would have a density of 2.5 to three units per acre, with a mixture of single-family and multifamily units. Mr. Risinger said the Comprehensive Plan recommends a gross density of one to four units per acre in LDR areas if particular public benefits are provided. He noted that single-family and multifamily units are recommended uses in LDR areas.

Mr. Risinger stated that the applicant has requested this item be placed on the Committee's Agenda to have a high-level discussion of the project and seek input and questions from the DRC members before starting detailed design work. He said no action by the DRC is required. Mr. Risinger said he would be happy to answer questions, and the applicant has a presentation available.

Mr. Greer addressed the Committee and said he appreciated the opportunity to meet with the Committee for its feedback on the design principles and acceptable density range of the proposed development. He said the estate owners contacted him for help understanding the land use designation change from Moderate Density Residential to LDR during the recent Comprehensive Plan update. He said the estate needs to sell and was looking for advice on the property. He said he grew up appreciating design with nature developments such as Kingsmill and supports the slow growth mindset.

Mr. Greer displayed an initial civil engineer drawing of what he said could be developed on the property. He said it did not consider the rural procession down Oakland Drive that the residents in the rear Oakland subdivision have enjoyed for many years. He noted that with the approved apartments next door, it is an emerging growth area but with a rural context. Mr. Greer said if the Comprehensive Plan acknowledges the need to account for growth in the area, then a development plan should still try to account for the history and layout of the property. He said the Oakland subdivision in the back has a covenant for no lots under one acre, and the residents enjoy the open field setting. Mr. Greer said the intent by the estate, however, was always to have higher density in the front. He said there is the possibility to do a Traditional Neighborhood Development or village-influenced design on the property, but he thought that would squeeze existing residents into a more urban setting. He said a different approach would consider the layout of the fields and clusters of specimen trees and preserve the rural character along Route 60 and the roadway procession to the existing community.

Mr. Greer then showed the Committee photos of the existing property, trees, and road procession to the Oakland subdivision. He showed an alternative design for the development which incorporates the existing Oakland Drive and tree clusters. He asked the Committee for its feedback on the best density range.

Mr. Polster said he is concerned about the impacts of the proposed density of 270 new units, in addition to the 126 approved apartment units and the existing 40-50 homes in the back. He stressed there were traffic mitigation concerns for the Richmond Road and Croaker Road corridors. Mr. Polster also stated that any new design should fit the aesthetics of the planned landscaping, roadways, and bikeways of the apartment development. Mr. Polster said he envisioned R-1, Limited Residential zoning for the property but suggested the applicant consider the impacts and what design guidelines, benefits, or proffers might be proposed to offset another proposed zoning.

Mr. Greer said he would look closer at the character design guidelines. He said they have spoken to traffic consultants about the circumstances and constraints and would research possible mitigation approaches to the traffic.

Ms. Null noted the Board of Supervisors has already received comments on the proposal from concerned citizens. She expressed concern about the proposed density, the traffic impacts, and possible requirements for additional schools.

Mr. Greer asked what would be considered preservation of the rural character.

Mr. Polster suggested limiting zoning to R-1, Limited Residential with one-acre lot density and a cap on traffic impacts to keep the same Level of Service.

Mr. Crump suggested looking at the net developable area when considering density. He noted the Ordinance update process is just beginning.

Mr. Greer said it is helpful to know what is desired by the community when considering proposed proffers, such as open space and trails, or affordable housing.

Mr. Rose commented the existing community will likely be concerned with the new development and fencing.

Mr. Greer said he understood and hopes to please the existing residents by keeping the existing roadway and offering a trail network potentially.

Mr. Wysong offered to send examples of recent residential rezonings for context and consideration of possible public benefits.

Mr. Haldeman asked about the driveways for proposed lots near the entrance.

Mr. Greer replied they would likely use a slip road.

Ms. Null suggested garages in the back.

Mr. Risinger noted that lots cannot front on existing roads within a major subdivision. He also noted there may be options in each zoning district for smaller lots to allow more open space, if a gross density is met and depending on water and sewer service. He also noted the cluster overlay is available in R-1 and R-2 Zoning Districts, but the R-1 Zoning District does not allow multifamily housing.

Mr. Polster suggested exploring the idea of rural clustering with staff.

Mr. Greer mentioned the concept of rural hamlets in Loudoun County, Virginia.

Mr. Polster also noted that extended Resource Protection Area buffers could be considered a public benefit. He said trails could also be located in that area.

Mr. Greer asked if keeping the existing Oakland Drive layout is the right solution.

Mr. Polster replied that it would depend on the settings and what development is proposed with it. He said most important would be the density, traffic, and aesthetics.

Mr. Greer thanked the Committee for its time and feedback.

Ms. Null thanked everyone for attending the meeting.

F. ADJOURNMENT

Mr. Rose motioned to Adjourn the meeting.

Ms. Null adjourned the meeting at 5:00 p.m. after a unanimous voice vote of 4-0.

Ms. Barbara Null, Chair

Mr. Paul Holt, Secretary

ITEM SUMMARY

DATE: 8/24/2022

TO: The Development Review Committee

FROM: Jose Ribeiro, Senior Planner II & Landscape Planner

SUBJECT: SP-22-0005. Colonial Veterinary Clinic Parking Expansion

ATTACHMENTS:

	Description	Type
☐	1. Staff Report	Staff Report
☐	2. Location Map	Exhibit
☐	3. Site Plan	Backup Material
☐	4. Adopted Master Plan	Backup Material
☐	5. Adopted Proffers	Backup Material
☐	6. Primary Principles for Five Forks Area	Backup Material

REVIEWERS:

Department	Reviewer	Action	Date
Development Review Committee	Crump, Josh	Approved	8/17/2022 - 9:55 AM
Development Review Committee	Secretary, DRC	Approved	8/17/2022 - 2:40 PM

SITE PLAN-22-0005. 3280 Ironbound Road, Colonial Veterinary Clinic Parking Expansion - Master Plan Consistency

Staff Report for the August 24, 2022, Development Review Committee Meeting

SUMMARY FACTS

Applicant: Mr. Ryan Stephenson of AES Consulting Engineers

Land Owner: Odlepahquote, LLC

Proposal: Development of an off-site parking area of 15 spaces for an existing veterinary clinic at a location previously identified on a master plan for the development of a 1,440-square-foot, two-story office building, a 520-square-foot attached garage, and a parking area with three spaces

Locations: 3280 Ironbound Road (existing veterinary clinic)
118 Ingram Road (undeveloped, proposed location for sidewalk)
112 Ingram Road (undeveloped, proposed location for additional off-site parking spaces)

Tax Map/Parcel Nos.: 4710100022B
4710100022A
4710100023

Project Acreage: +/- 1.2 acres
+/- 0.8 acres
+/- 0.3 acres
+/- 2.3 acres

Zoning: B-1, General Business, with proffers

Comprehensive Plan: Mixed Use

Primary Service Area: Inside

Staff Contact: Jose Ribeiro, Senior Planner II

REASON FOR PLANNING COMMISSION REVIEW FACTORS FAVORABLE

1. Staff finds the proposal will not negatively impact surrounding development.
2. Standalone parking areas are permitted in B-1 Zoning Districts.

FACTORS UNFAVORABLE

1. Staff finds that the proposed use of the property is not consistent with the use approved as part of a previous rezoning (Z-0005-2007).

SUMMARY STAFF RECOMMENDATION

Staff recommends that the Development Review Committee (DRC) find this proposal not consistent with the adopted master plan. Staff also recommends the DRC recommend denial to the Planning Commission and that the applicant pursue a master plan amendment through the legislative application process.

PROJECT HISTORY

- Colonial Veterinary Clinic is located at 3280 Ironbound Road at the intersection of Ironbound and Ingram Roads.

This staff report is prepared by the James City County Planning Division to provide information to the Planning Commission and Board of Supervisors to assist them in making a recommendation on this application. It may be useful to members of the general public interested in this application.

SITE PLAN-22-0005. 3280 Ironbound Road, Colonial Veterinary Clinic Parking Expansion - Master Plan Consistency

Staff Report for the August 24, 2022, Development Review Committee Meeting

- In 1997, a site plan (SP-21-97) was approved for the construction of a 4,100-square-foot building along with a parking lot with capacity for 16 parking spaces.
- On February 12, 2008, the Board of Supervisors approved a rezoning request for property located at 112 Ingram Road (Z-0005-2007) from R-8, Rural Residential, to B-1, General Road Business, allowing for the construction of a two-story office building, attached garage, and three parking spaces.
- In 2019, a site plan amendment (SP-19-0120) for the installation of an outdoor generator at the veterinary clinic was approved by staff.
- The properties located at 112 and 118 Ingram Road have remained undeveloped over the years.

PROJECT DESCRIPTION

- The current site plan proposes the development of a parking area and pedestrian facilities accessory to the existing veterinary clinic but located off-site at 118 and 112 Ingram Road. According to the applicant, the additional parking spaces provided will allow for additional parking to the staff of the clinic. The parking requirements were met for the clinic during the initial site plan.
- All three properties share the same zoning and Comprehensive Plan designations: B-1 and Mixed Use; and ownership.
- As part of the rezoning of the property at 112 Ingram Road, a request for a reduction of the front setback from 50 feet to 25 feet and a reduction of the width of the landscape buffer along Ingram Road from 30 feet to 20 feet were also approved.

SURROUNDING ZONING AND DEVELOPMENT

- North, South, East, and West: Properties are zoned B-1, General Business.

STAFF ANALYSIS

At the time of the rezoning application there was a deliberate effort made by the applicant and staff to ensure that the property developed in accordance with the Primary Principles of the Five Forks Area of James City County. Adopted by the Board of Supervisors in 2004, these are principles designed to guide future development in the Five Forks area such as preserving the area’s unique village character.

A proffer for architecture (Proffer No. 2) required the office building to be developed in an architectural design and color scheme that was consistent with the architectural standards set forth by the principles of the Five Forks area. As part of the standards, proposed parking areas would be located behind proposed buildings. Enhanced landscaping along the property’s frontage was also proffered (Proffer No. 3). Staff finds that the original use of the property, as approved as part of the rezoning, to be more consistent and in keeping with the principles of Five Forks than a stand-alone parking area.

Further, Proffer No. 1 of the adopted rezoning (Z-0005-2007) requires that development of the property be similar to the approved master plan:

“The Property shall be put to one or more of the following possible uses: business, governmental, contractor’s and professional offices and accessory uses thereto as defined in the James City County Zoning Ordinance. All other otherwise permissible uses shall be prohibited. The Property shall be developed generally in accordance with the Plan, with such minor changes as the Development Review Committee

This staff report is prepared by the James City County Planning Division to provide information to the Planning Commission and Board of Supervisors to assist them in making a recommendation on this application. It may be useful to members of the general public interested in this application.

SITE PLAN-22-0005. 3280 Ironbound Road, Colonial Veterinary Clinic Parking Expansion - Master Plan Consistency

Staff Report for the August 24, 2022, Development Review Committee Meeting

determines does not change the basic concept or character of the development.”

Staff finds that the current proposal does change the basic concept and character of the development from the approved master plan.

STAFF RECOMMENDATION

Staff recommends that the DRC find this proposal not consistent with the adopted master plan. Staff also recommends the DRC recommend denial to the Planning Commission and that the applicant pursue a master plan amendment through the legislative application process.

JR/md

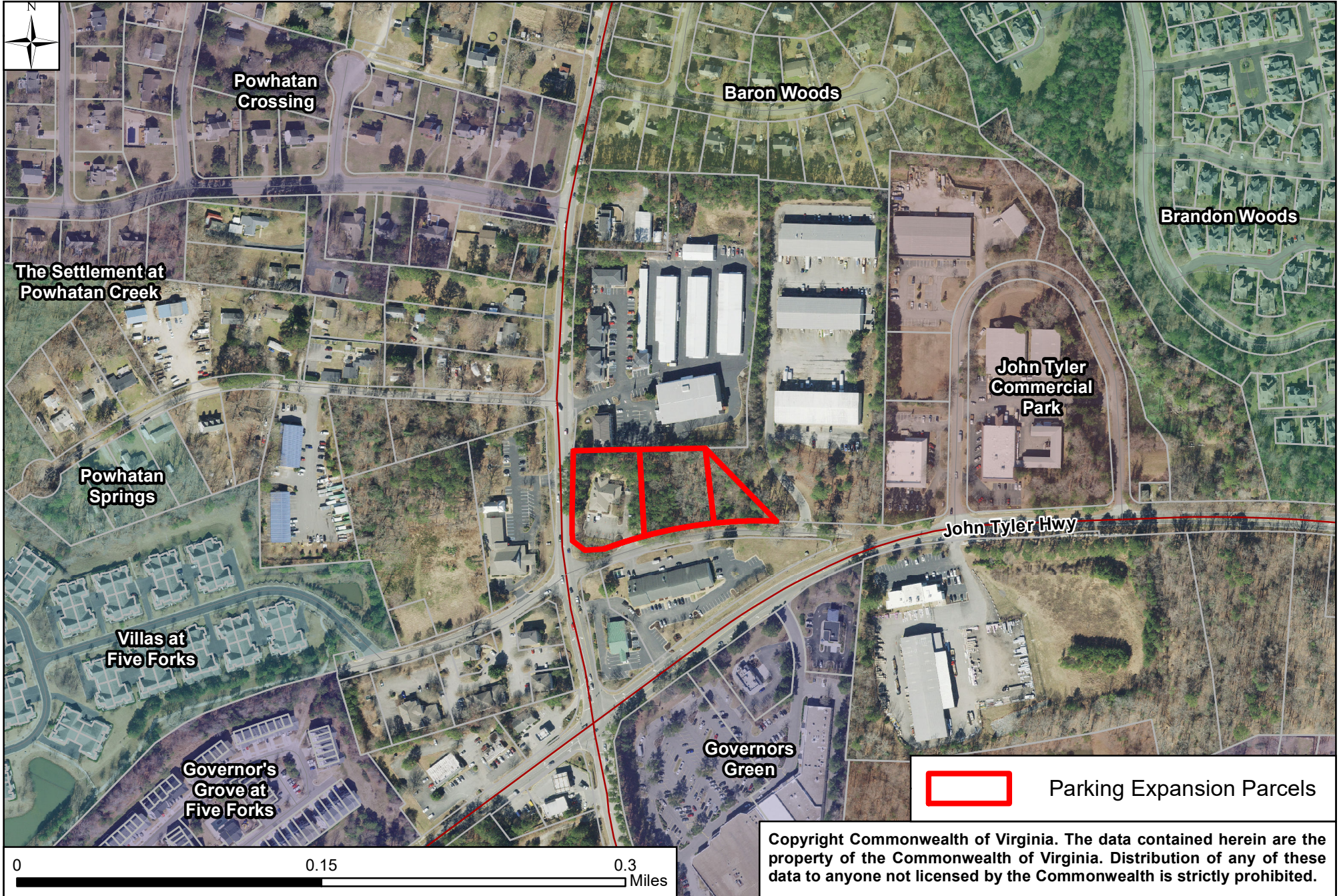
SP22-5-3280IrnbdExp

Attachments:

1. Location map
2. Current Site Plan
3. Adopted Master Plan
4. Adopted Proffers
5. Primary Principles of the Five Forks Area

This staff report is prepared by the James City County Planning Division to provide information to the Planning Commission and Board of Supervisors to assist them in making a recommendation on this application. It may be useful to members of the general public interested in this application.

JCC SP-22-0005, 3280 Ironbound Rd. Colonial Veterinary Clinic Parking Expansion

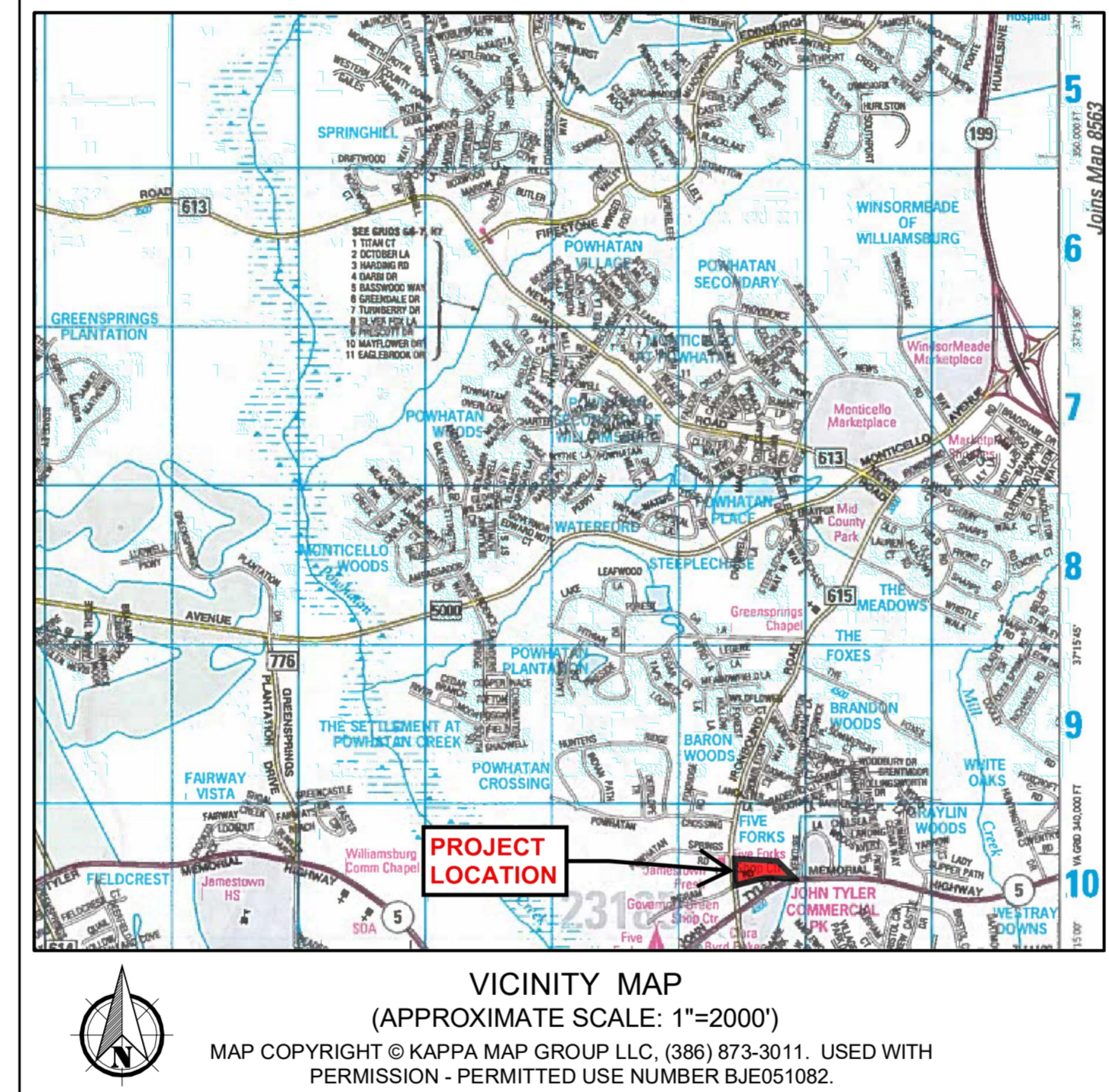


GENERAL NOTES:

- THIS SITE LIES WITHIN THE JAMES RIVER - CHIPPOKES CREEK WATERSHED.
- ALL ERRORS OR DISCREPANCIES WITH THE PLANS OR EXISTING SITE CONDITIONS SHALL BE REPORTED TO THE ENGINEER OR SURVEYOR OF RECORD BEFORE PROCEEDING WITH THE WORK.
- CONTOUR INTERVAL IS 1 FOOT. EXISTING GRADE IS FROM A COMBINATION OF THE FOLLOWING: A FIELD SURVEY PROVIDED BY AES CONSULTING ENGINEERS DATED AUGUST 2021 AND JAMES CITY COUNTY GIS.
- ALL UTILITY AND SURVEY DATA SHOWN ON THE DRAWINGS HAS BEEN PROVIDED BY THE COMBINATION OF SURVEYS AS DESCRIBED ABOVE. INFORMATION HAS BEEN OBTAINED FROM THE BEST AVAILABLE SOURCES AT THE TIME OF THE SURVEY PROVIDED BY AES CONSULTING ENGINEERS BUT IS NOT REPRESENTED AS BEING COMPLETE AND ACCURATE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO LOCATE AND PROTECT EXISTING UTILITIES AND UNDERGROUND STRUCTURES. DAMAGE TO EXISTING UTILITIES AND UNDERGROUND STRUCTURES SHALL BE REPAIRED BY THE CONTRACTOR TO NO ADDITIONAL COST TO THE DEVELOPER.
- EXISTING UTILITY LOCATIONS INDICATED ARE APPROXIMATE. FIELD VERIFY PRIOR TO COMMENCING THE WORK.
- OWNER SHALL OBTAIN A LAND DISTURBING PERMIT, WITH SURETY, IS REQUIRED FOR THIS PROJECT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN ON THE DRAWINGS AND FOR EXCAVATION, STOCKPILES, STAGING AREAS, MOBILIZATION SITES, BEDDING/BACKFILL STOCKPILES, AND OTHER LAND DISTURBANCES NOT SPECIFICALLY ADDRESSED IN THE DRAWINGS OR CONTRACT DOCUMENTS. EROSION AND SEDIMENT CONTROL MEASURES SHALL MEET OR EXCEED THE MINIMUM STANDARDS OF THE "VIRGINIA EROSION AND SEDIMENT CONTROL HANDBOOK" (LATEST PUBLICATION) AND THE REQUIREMENTS OF THE LOCAL GOVERNING AUTHORITY.
- ALL DISTURBED AREAS, INCLUDING BUT NOT LIMITED TO PAVEMENT, SHOULDERS, DITCHES, HEADWALLS, ENDWALLS, CULVERT PIPES, CURB AND GUTTER, UTILITIES, DRIVEWAYS, SIGNS, MAILBOXES, ETC., SHALL BE REPAIRED TO A CONDITION EQUAL TO OR BETTER THAN THOSE EXISTING PRIOR TO CONSTRUCTION OR AS SHOWN ON THE DRAWINGS. SIGNS, MAILBOXES, AND GUARDRAILS THAT ARE DISTURBED SHALL BE RETURNED TO THEIR ORIGINAL LOCATIONS DAILY AND MAINTAINED THROUGHOUT THE PROJECT.
- THE CONTRACTOR SHALL REESTABLISH ALL PROPERTY PINS, MONUMENTS, WATER METERS, DRAINAGE CULVERTS, FENCES, UTILITY POLES, DRIVEWAYS, CURBS, GUTTERS, ETC. DISTURBED DURING CONSTRUCTION AT NO ADDITIONAL COST TO THE DEVELOPER/OWNER.
- ALL PROPOSED UTILITIES SHALL BE PLACED UNDERGROUND AS PER JAMES CITY COUNTY ZONING ORDINANCE SECTION 24-200.
- PRIVATELY OWNED UTILITIES SHOWN ON THIS PLAN ARE REGULATED BY THE VIRGINIA UNIFORM STATEWIDE BUILDING CODE AND ENFORCED BY THE CODE COMPLIANCE DIVISION. THESE PRIVATELY OWNED UTILITIES MUST COMPLY FULLY WITH THE INTERNATIONAL PLUMBING CODE, THE NATIONAL FIRE PREVENTION ASSOCIATION STANDARD 24, AND THE INTERNATIONAL FIRE CODE. CONTRACTORS WORKING FROM THIS SITE PLAN ARE CAUTIONED NOT TO INSTALL OR CONCEAL PRIVATELY OWNED SITE UTILITIES WITHOUT OBTAINING REQUIRED PERMITS AND INSPECTIONS.
- CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND NOTIFY THE OWNER OF ANY DISCREPANCIES PRIOR TO AND DURING CONSTRUCTION.
- ANY EXISTING, UNUSED WELLS SHALL BE ABANDONED IN ACCORDANCE WITH THE VIRGINIA PRIVATE WELL REGULATIONS AND JAMES CITY COUNTY CODE.
- THE CONTRACTOR SHALL MAINTAIN A COMPLETE SET OF THE APPROVED PLANS AT THE PROJECT SITE AT ALL TIMES DURING CONSTRUCTION.
- CONTRACTORS SHALL NOTIFY MISS UTILITY @ 811 OR (800) 552-7001 PRIOR TO STARTING CONSTRUCTION. CONTRACTOR IS RESPONSIBLE FOR LOCATING AND DETERMINING SIZES OF ALL UTILITIES PRIOR TO CONSTRUCTION. IF CONDITIONS DIFFER FROM THOSE SHOWN ON THE PLANS, CONTACT THE ENGINEER BEFORE PROCEEDING.
- THE CONTRACTOR SHALL PROVIDE WRITTEN NOTIFICATION TO ALL OWNERS AND RESIDENTS OF PROPERTY ADJACENT TO THE PROJECT 30 DAYS PRIOR TO THE COMMENCEMENT OF WORK, UNLESS OTHERWISE DIRECTED BY THE OWNER. CONSTRUCTION WITHIN EASEMENTS OR ON PUBLIC RIGHT-OF-WAY NECESSITATES NOTICE WHETHER ADJACENT TO OR LOCATED ON THE ADJOINING PROPERTY. FAILURE TO PROVIDE THE MINIMUM NOTIFICATION TIME WILL RESULT IN SUSPENSION OF WORK.
- THE ABSENCE OF THE OWNER OR THE ENGINEER AT THE JOB SITE DOES NOT, IN ANY WAY, RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY TO PERFORM THE WORK IN ACCORDANCE WITH THE DRAWINGS, CONTRACT DOCUMENTS, ADDENDA, AND WRITTEN AUTHORIZED PLAN REVISIONS.
- THE CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE LAWS, ORDINANCES, RULES, REGULATIONS, AND ORDERS OF ANYBODY HAVING JURISDICTION. THE CONTRACTOR SHALL ERECT AND MAINTAIN, AS REQUIRED BY THE CONDITIONS AND PROGRESS OF THE WORK, ALL NECESSARY SAFEGUARDS FOR SAFETY AND PROTECTION.
- ALL CONSTRUCTION METHODS AND MATERIALS SHALL CONFORM WITH THE CURRENT JAMES CITY COUNTY STANDARDS AND SPECIFICATIONS, VIRGINIA DEPARTMENT OF TRANSPORTATION ROAD AND BRIDGE STANDARDS AND SPECIFICATIONS, VIRGINIA EROSION AND SEDIMENT CONTROL REGULATIONS, AND ANY OTHER APPLICABLE CITY OR STATE ORDINANCES, CODES, AND LAWS PRIOR TO ANY CONSTRUCTION ACTIVITY.
- THE CONTRACTOR SHALL USE ONLY NEW MATERIALS, PARTS AND PRODUCTS ON ALL PROJECTS. ALL MATERIALS SHALL BE STORED SO AS TO ASSURE THE PRESERVATION OF THEIR QUALITY AND FITNESS FOR THE WORK.
- THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS PRIOR TO COMMENCEMENT OF WORK INCLUDING, BUT NOT LIMITED TO, LAND DISTURBING, BUILDING, RIGHT-OF-WAY, AND UTILITY PERMITS.
- CONTRACTOR SHALL BE RESPONSIBLE FOR THE COORDINATION OF CONSTRUCTION EFFORTS WITH LOCAL EMERGENCY SERVICES AND ALL NECESSARY UTILITY COMPANIES INCLUDING, BUT NOT LIMITED TO, ELECTRICITY, NATURAL GAS, TELECOMMUNICATIONS, CABLE TELEVISION, WATER, SEWER, PRIVATE LIGHTING, AND OTHERS THAT MAY BE REQUIRED.
- THE CONTRACTOR SHALL SATISFY HIMSELF AS TO ALL SITE CONDITIONS PRIOR TO CONSTRUCTION.
- THE CONTRACTOR IS RESPONSIBLE FOR MAINTENANCE OF ALL SITE IMPROVEMENTS, INCLUDING LANDSCAPING, AS SHOWN ON THE APPROVED PLAN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ESTABLISHING AND MAINTAINING ALL LINES AND GRADES REQUIRED.
- THE CONTRACTOR SHALL REMOVE ALL EXCESS MATERIAL, INCLUDING SOIL AND DEBRIS, FROM THE SITE. ALL OBJECTIONABLE AND DELETERIOUS MATERIAL IS TO BE REMOVED FROM THE SITE AND DISPOSED OF IN A STATE APPROVED FACILITY MEETING THE REQUIREMENTS OF ALL APPLICABLE LOCAL, STATE, AND FEDERAL REGULATIONS.
- THE CONTRACTOR SHALL COMPLY WITH ALL PROVISIONS OF THE VIRGINIA UNDERGROUND UTILITY DAMAGE PREVENTION ACT (SECTION 56-265.14 ET. SEQ. CODE OF VIRGINIA, 1950, AS AMENDED) AND HEREBY AGREES TO HOLD THE DEVELOPER AND THE ENGINEER HARMLESS AGAINST ANY LOSS, DAMAGE, OR CLAIMS OF ANY NATURE WHATSOEVER ARISING OUT OF THE CONTRACTOR'S FAILURE TO COMPLY WITH THE REQUIREMENTS OF SAID ACT. THE CONTRACTOR IS REQUIRED TO COMPLY WITH THE VIRGINIA OVERHEAD HIGH VOLTAGE LINE SAFETY ACT (SECTIONS 59-1406 THROUGH 59-1414, CODE OF VIRGINIA, 1950, AS AMENDED). THE CONTRACTOR IS REQUIRED TO VISIT THE SITE AND NOTE THE POSITION OF OVERHEAD CABLES PRIOR TO CONSTRUCTION.
- HORIZONTAL DATUM: NAD83 - VA SOUTH ZONE 4502
VERTICAL DATUM: NGVD29
- THE PROFESSIONAL WHOSE SEAL IS AFFIXED HEREON SHALL ACT AS THE "RESPONSIBLE LAND DISTURBER" FOR PURPOSES OF PLAN APPROVAL ONLY. PRIOR TO ISSUANCE OF THE LAND DISTURBING PERMIT, THE CONTRACTOR SHALL PROVIDE THE NAME OF A "RESPONSIBLE LAND DISTURBER" WHO SHALL ASSUME RESPONSIBILITY AS THE "RESPONSIBLE LAND DISTURBER" FOR THE CONSTRUCTION PHASE OF THE PROJECT. THE CONTRACTOR SHALL PROVIDE WRITTEN NOTIFICATION SHOULD THE "RESPONSIBLE LAND DISTURBER" CHANGE DURING CONSTRUCTION.
- STORM SEWER PIPE SHALL BE CLASS III RCP OR SMOOTH WALL INTERIOR HDPE PIPE UNLESS OTHERWISE DESIGNATED. RCP SHALL BE USED IN ALL LOCATIONS UNDER CURB AND GUTTER.
- STORM STRUCTURES SHALL CONFORM TO THE CURRENT VDOT ROAD AND BRIDGE STANDARDS AND VDOT SPECIFICATIONS. ALL MANHOLES SHALL INCLUDE INLET SHAPING (IS-1) AND MANHOLES DEEPER THAN 4 FEET SHALL HAVE STEPS (ST-1). PIPE BEDDING SHALL BE IN ACCORDANCE WITH VDOT PB-1 AND MANUF. SPECS/GUIDELINES.
- ALL FILL MATERIAL SHALL BE VOID OF DEBRIS AND PLACED IN UNIFORM LIFTS OF NOT MORE THAN 8" IN LOOSE DEPTH. COMPACTION SHALL BE PERFORMED WITH HEAVY COMPACTION EQUIPMENT. COMPACT TO 95% MAXIMUM DRY DENSITY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING COMPACTION TEST RESULTS BY A GEOTECHNICAL ENGINEER TO THE OWNER/DEVELOPER FOR VERIFICATION OF PROPER COMPACTION.
- ALL TRAFFIC CONTROL SIGNS AND PAVEMENT MARKINGS SHALL CONFORM TO THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) FOR STREETS AND HIGHWAYS.
- ALL PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH DETAILS AND NOTES.
- THE CONTRACTOR SHALL STRICTLY ADHERE TO THE CLEARING LIMITS AS SHOWN ON THE APPROVED PLAN. IF IT IS NECESSARY TO CLEAR TREES BEYOND THE LIMITS OF DISTURBANCE, THE CONTRACTOR SHALL CONTACT THE DESIGN ENGINEER AND COORDINATE THE WORK PRIOR TO ANY CLEARING.
- ALL LIGHTING WILL BE IN COMPLIANCE WITH ARTICLE II, DIVISION 7 OF THE ZONING ORDINANCE.
- ALL NEW SIGNS SHALL BE SET IN ACCORDANCE WITH ARTICLE II, DIVISION 3 OF THE JAMES CITY COUNTY ZONING ORDINANCE.
- COORDINATION INCLUDES BEING THE PARTY RESPONSIBLE TO COORDINATE WITH DEVELOPER/OWNER TO ENSURE THAT OTHER UTILITIES (PUBLIC OR PRIVATE) DO NOT VIOLATE THE JCSA POLICY OF EXCLUSIVE OCCUPATION OF JCSA EASEMENTS OTHER THAN PERPENDICULAR CROSSINGS.
- THE PROPERTY SHALL BE SERVED BY PUBLIC WATER AND PRIVATE SEWER.
- ALL NEW MONUMENTS SHALL BE SET IN ACCORDANCE WITH SEC. 19-34 THROUGH 19-36 OF THE JAMES CITY COUNTY SUBDIVISION ORDINANCE.
- SUBMISSION, REVIEW, AND APPROVAL OF RECORD DRAWINGS (AS-BUILTS) AND CONSTRUCTION CERTIFICATIONS FOR THE STORMWATER CONVEYANCE SYSTEMS AND STORMWATER MANAGEMENT FACILITY ARE REQUIRED PRIOR TO RELEASE OF THE POSTED BOND/SURETY. CONTRACTOR SHALL ENSURE THIS ACTIVITY IS ADEQUATELY COORDINATED AND PERFORMED BEFORE, DURING, AND FOLLOWING CONSTRUCTION IN ACCORDANCE WITH CURRENT COUNTY GUIDELINES.
- AN INTERNAL CLOSED CIRCUIT TELEVISION (CCTV) POST INSTALLATION INSPECTION PERFORMED BY THE OPERATOR IS REQUIRED FOR ALL STORMWATER CONVEYANCE SYSTEM PIPES, ACCESS OR INLET STRUCTURES, AND CULVERTS OF 15-INCH NOMINAL DIAMETER SIZE OR GREATER AS PART OF THE CONSTRUCTION RECORD DRAWING (AS-BUILT) AND CONSTRUCTION CERTIFICATION PROCESS. CCTV INSPECTIONS SHALL BE SUBMITTED ON A CD-ROM OR EQUIVALENT ELECTRONIC FILE FORMAT FOR STAFF REVIEW. REFER TO SECTIONS 8-25(f) AND 2-27(e) OF THE COUNTY'S CHAPTER 8 ORDINANCE.
- SETBACKS PER B-1 ZONING REQUIREMENTS:
 - FRONT SETBACK: STRUCTURES SHALL BE LOCATED 50 FEET OR MORE FROM ANY STREET R/W WHICH IS 50 FEET OR GREATER IN WIDTH
 - SIDE YARD: 20 FEET
 - REAR YARD: 20 FEET

SITE PLAN FOR COLONIAL VETERINARY CLINIC PARKING EXPANSION

JAMESTOWN DISTRICT JAMES CITY COUNTY VIRGINIA



COUNTY SITE PLAN NO.: SP-22-0005
ORIGINAL SUBMITTAL DATE: 01/12/2022

LEGEND

EXISTING	PROPOSED	EXISTING	PROPOSED
EX. WATER	WATER	--- ---	CENTERLINE/BASELINE
EX. SAN.	SANITARY SEWER	--- ---	RIGHT OF WAY
EX. FORCE MAIN	STORM SEWER	--- ---	PROPERTY LINE
	FORCE MAIN	--- ---	& DITCH/SWALE
	SANITARY MANHOLE	--- ---	CONCRETE LINED DITCH
	STORM MANHOLE	--- ---	EXISTING TREELINE
	CURB DROP INLET	--- ---	LIMITS OF CLEARING
	YARD DROP INLET	--- ---	RIP RAP
	FLARED END SECTION	--- ---	CURB
	VALVE	--- ---	CURB AND GUTTER
	FIRE HYDRANT ASSEMBLY	--- ---	REVERSE GUTTER PAN
	BLOW-OFF VALVE	--- ---	EDGE OF PAVEMENT
	AIR RELEASE ASSEMBLY	--- ---	EXISTING GROUND ELEVATION
	CLEAN OUT	--- ---	PROPOSED SPOT GRADE
	WATER METER	--- ---	CONTOUR
	STREETLIGHT	--- ---	



- CONTRACTORS SHALL NOTIFY MISS UTILITY @ 811 OR (800) 552-7001 PRIOR TO STARTING CONSTRUCTION.
- CONTRACTOR IS RESPONSIBLE FOR LOCATING AND DETERMINING SIZES OF ALL UTILITIES PRIOR TO CONSTRUCTION.
- IF CONDITIONS DIFFER FROM THOSE SHOWN ON THE PLANS, CONTACT THE CONSTRUCTION MANGER BEFORE PROCEEDING.
- GOVERNMENT WILL MARK ALL GOVERNMENT-OWNED UTILITIES.

INDEX OF SHEETS	
SHEET NO.	SHEET DESCRIPTION
C1.0	COVER SHEET
C2.0	ENVIRONMENTAL INVENTORY
C3.0	DEMOLITION AND PHASE I E&S PLAN
C4.0	SITE AND UTILITY PLAN
C5.0	GRADING AND DRAINAGE PLAN
C6.0	E&S NOTES AND DETAILS
C6.1	SITE NOTES AND DETAILS
C6.2	TRAFFIC CONTROL NOTES AND DETAILS
L1.0	LANDSCAPE AND LIGHTING PLAN
L2.0	LANDSCAPING AND LIGHTING NOTES AND DETAILS

CERTIFIED RESPONSIBLE LAND DISTURBER:

T. RYAN STEPHENSON, P.E.
AES CONSULTING ENGINEERS
5248 OLDE TOWNE ROAD, SUITE 1
WILLIAMSBURG, VIRGINIA 23188
TELEPHONE: 757-253-0040

* FOR SITE PLAN REVIEW PROCESS ONLY. OWNER OR CONTRACTOR SHALL NAME RESPONSIBLE LAND DISTURBER FOR CONSTRUCTION PROCESS.

OWNER/DEVELOPER INFORMATION:

COLONIAL VETERINARY CLINIC

CONTACT: MS. RUTH REDBIRD
PRACTICE MANAGER
3280 IRONBOUND ROAD
WILLIAMSBURG, VIRGINIA 23188
PHONE: (757) 220-5589

PARKING CALCULATION:

REQUIRED PARKING:
3 PARKING SPACES PER EXAMINATION OR TREATMENT ROOM, PLUS ONE SPACE PER EMPLOYEE ON THE LARGEST SHIFT
(PER CHAPTER 24, ARTICLE II, DIVISION 2, SECTION 24-59 OF THE CODE OF ORDINANCES FOR JAMES CITY COUNTY, VIRGINIA)

2 HC PARKING SPACES PER 26 TO 50 TOTAL PARKING SPACES PROVIDED
(PER TABLE 208.2 OF THE 2010 ADA STANDARDS FOR ACCESSIBLE DESIGN)

5-EXAMINATION ROOMS = 33 PARKING SPACES REQUIRED
18-EMPLOYEES ON THE LARGEST SHIFT = (INCLUDING 2 HC PARKING SPACES)

EXISTING PARKING:
15 REGULAR PARKING SPACES
1 HC PARKING SPACES
= 19 TOTAL PARKING SPACES

PROVIDED PARKING:
15 ADDITIONAL REGULAR PARKING SPACES
18 EXISTING REGULAR PARKING SPACES
1 EXISTING HC PARKING SPACES
= 34 TOTAL PARKING SPACES

SITE DATA:

SITE ADDRESS: 3280 IRONBOUND ROAD, 118 INGRAM ROAD, 112 INGRAM ROAD, WILLIAMSBURG, VIRGINIA 23188
LATITUDE: 37° 15' 13" N; LONGITUDE: 76° 45' 55" W

ZONING: B-1, GENERAL BUSINESS

PARCEL ID: 4710100022B, 4710100022A, 4710100023

OVERALL SITE AREA: 97,710 S.F.±, 2.24 AC.±

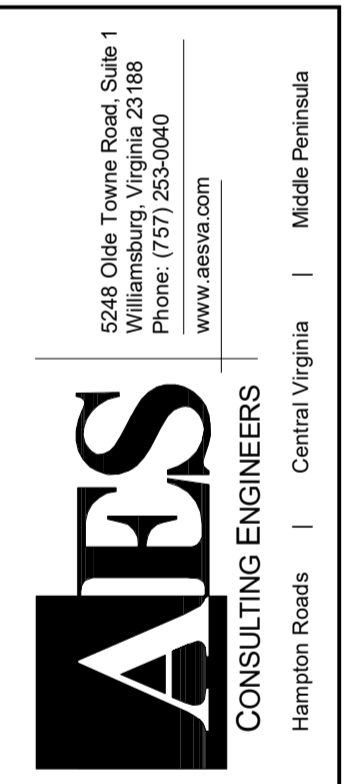
LIMITS OF DISTURBANCE: 17,253 S.F.±, 0.40 AC.±

PRE-IMPERVIOUS AREA: 0.00 S.F.±, 0.00 AC.±

POST-IMPERVIOUS AREA: 7,655 S.F.±, 0.18 AC.±

FLOOD HAZARD MAP: THIS PROPERTY IS LOCATED WITHIN FLOOD ZONE "X" AS SHOWN ON MAP NUMBER 51095C0119D EFFECTIVE ON 12/16/2015 OF THE FLOOD INSURANCE RATE MAPS FOR JAMES CITY COUNTY, VIRGINIA. ZONE "X" IS DEFINED AS AREAS OF MINIMAL FLOOD HAZARD.

Rev.	Date	Description
1	07/21/2022	REVISED PER 1ST SUBMITTAL ACC COMMENTS



SITE PLAN FOR
**COLONIAL VETERINARY
CLINIC PARKING EXPANSION**

JAMESTOWN DISTRICT JAMES CITY COUNTY VIRGINIA

Project Contacts: TRS
Project Number: W10705-00
Scale: Date: 01/12/2022
AS NOTED
Sheet Title: COVER SHEET
Sheet Number: **C1.0**

N OR F
JEANETTE B. BRADY, TRUSTEE
INSTRUMENT #150016086
JCC PARCEL ID #4710100026
ZONED: B1 GENERAL BUSINESS
INSTRUMENT #050024046 (PLAT)

LOT 2, C. JONES SUBD.
N OR F
ODLEPAHQOOTE, LLC
INSTRUMENT #210001661
JCC PARCEL ID #4710100022B
ZONED: B1 GENERAL BUSINESS
DEED BOOK 243, PAGE 407 (PLAT)

LOT 1, C. JONES SUBD.
36,893 S.F.
0.847 ACRES
N OR F
ODLEPAHQOOTE, LLC
INSTRUMENT #210001661
JCC PARCEL ID #4710100022A
ZONED: B1 GENERAL BUSINESS
DEED BOOK 243, PAGE 407 (PLAT)

#112 INGRAM RD
16,285 S.F.
0.374 ACRES
N OR F
ODLEPAHQOOTE, LLC
INSTRUMENT #210020293
JCC PARCEL ID #4710100023
ZONED: B1 GENERAL BUSINESS

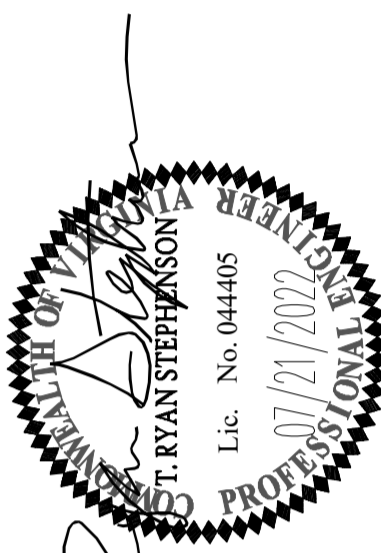
RESIDUE HENRY ARMISTEAD ESTATE
N OR F
INGRAM ROAD, LLC,
C/O WHH TRICE & CO.
INSTRUMENT #000010647
JCC PARCEL ID #4710100007
ZONED: B1 GENERAL BUSINESS
PLAT BOOK 43, PAGE 95

BUILDING FOUNDATION
TAKEN FROM SURVEY BY AES,
PROJECT #8252-00 AND
FITTED INTO THIS SURVEY
(NOT PART OF THIS SURVEY)

JAMES CITY COUNTY, PRIMARY SERVICE AREA
GEODETIC GROUND CONTROL NETWORK - JULY 1990
VIRGINIA STATE PLANE COORDINATES
SOUTH ZONE 4502 (NAD83)(1986)
MONUMENT STATION #321, RESET 1990

HORIZONTAL DATUM:
JAMES CITY COUNTY, PRIMARY SERVICE AREA
GEODETIC GROUND CONTROL NETWORK JULY 1990,
VIRGINIA STATE PLANE COORDINATES, SOUTH ZONE 4502
NORTH AMERICAN DATUM OF 1983 (NAD83)(1986)
MONUMENT STATION #321, RESET 1990

VERTICAL DATUM:
JAMES CITY COUNTY, PRIMARY SERVICE AREA
GEODETIC GROUND CONTROL NETWORK JULY 1990,
NATIONAL GEODETIC VERTICAL DATUM OF 1929 = (NGVD29)
MONUMENT STATION #321, RESET 1990



THIS PLAN HAS
NOT RECEIVED
FINAL APPROVAL
AND IS NOT
APPROVED FOR
CONSTRUCTION.

5248 Old Towne Road, Suite 1
Williamsburg, Virginia 23188
Phone: (757) 255-0640
www.aes.com

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CONSULTING ENGINEERS

Hampton Roads | Central Virginia | Middle Peninsula

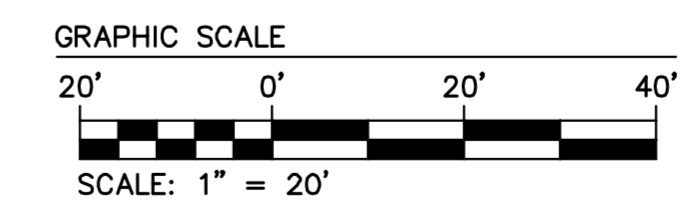
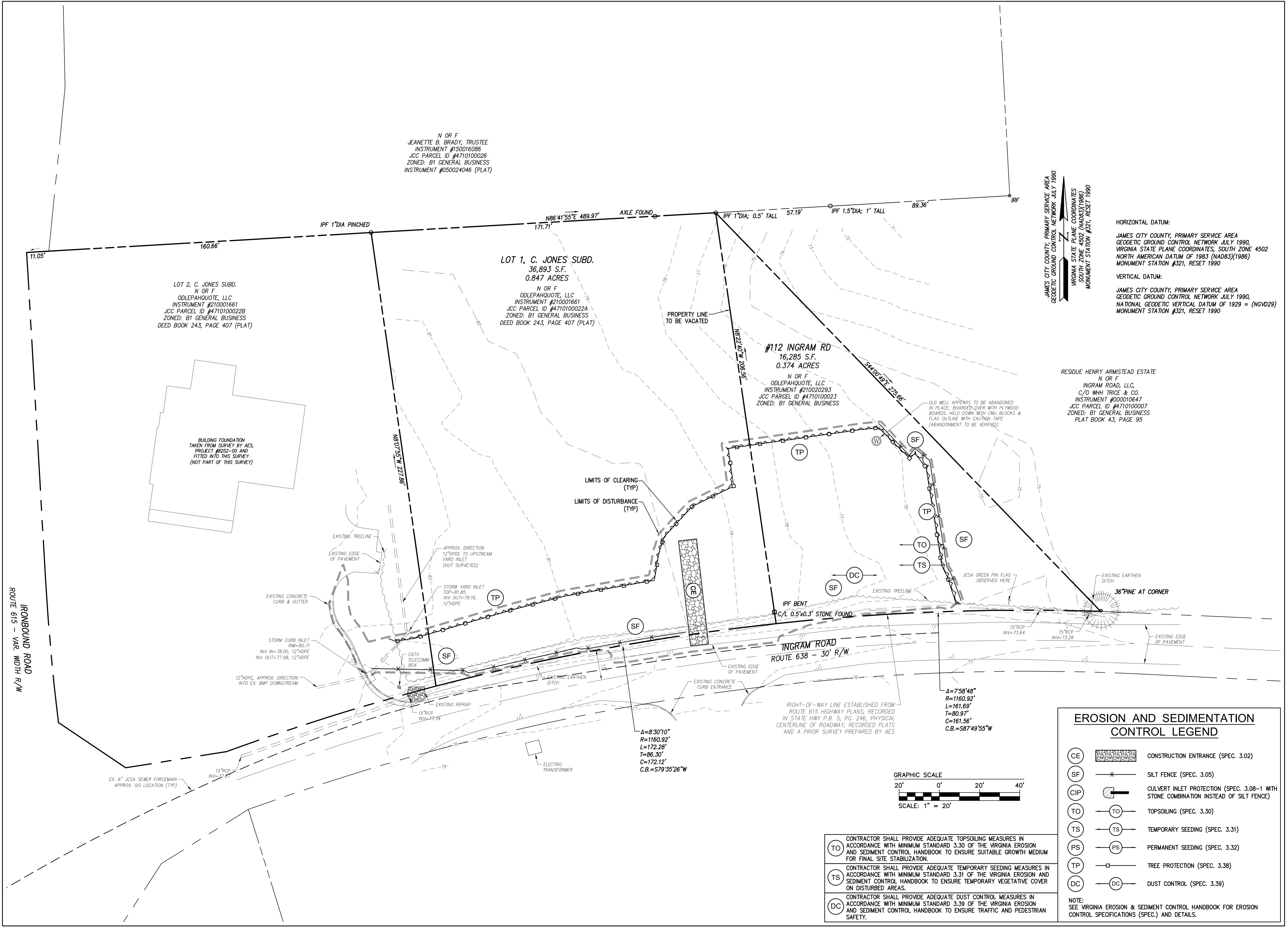
SITE PLAN
FOR
**COLONIAL VETERINARY
CLINIC PARKING EXPANSION**

JAMES CITY COUNTY
JAMESTOWN DISTRICT

Project Contacts: TRS
Project Number: W10705-00
Scale: 1"=20' Date: 01/12/2022

Sheet Title:
**DEMOLITION AND
PHASE I E&S
PLAN**

Sheet Number
C3.0



- (TO) CONTRACTOR SHALL PROVIDE ADEQUATE TOPSOILING MEASURES IN ACCORDANCE WITH MINIMUM STANDARD 3.30 OF THE VIRGINIA EROSION AND SEDIMENT CONTROL HANDBOOK TO ENSURE SUITABLE GROWTH MEDIUM FOR FINAL SITE STABILIZATION.
- (TS) CONTRACTOR SHALL PROVIDE ADEQUATE TEMPORARY SEEDING MEASURES IN ACCORDANCE WITH MINIMUM STANDARD 3.31 OF THE VIRGINIA EROSION AND SEDIMENT CONTROL HANDBOOK TO ENSURE TEMPORARY VEGETATIVE COVER ON DISTURBED AREAS.
- (DC) CONTRACTOR SHALL PROVIDE ADEQUATE DUST CONTROL MEASURES IN ACCORDANCE WITH MINIMUM STANDARD 3.39 OF THE VIRGINIA EROSION AND SEDIMENT CONTROL HANDBOOK TO ENSURE TRAFFIC AND PEDESTRIAN SAFETY.

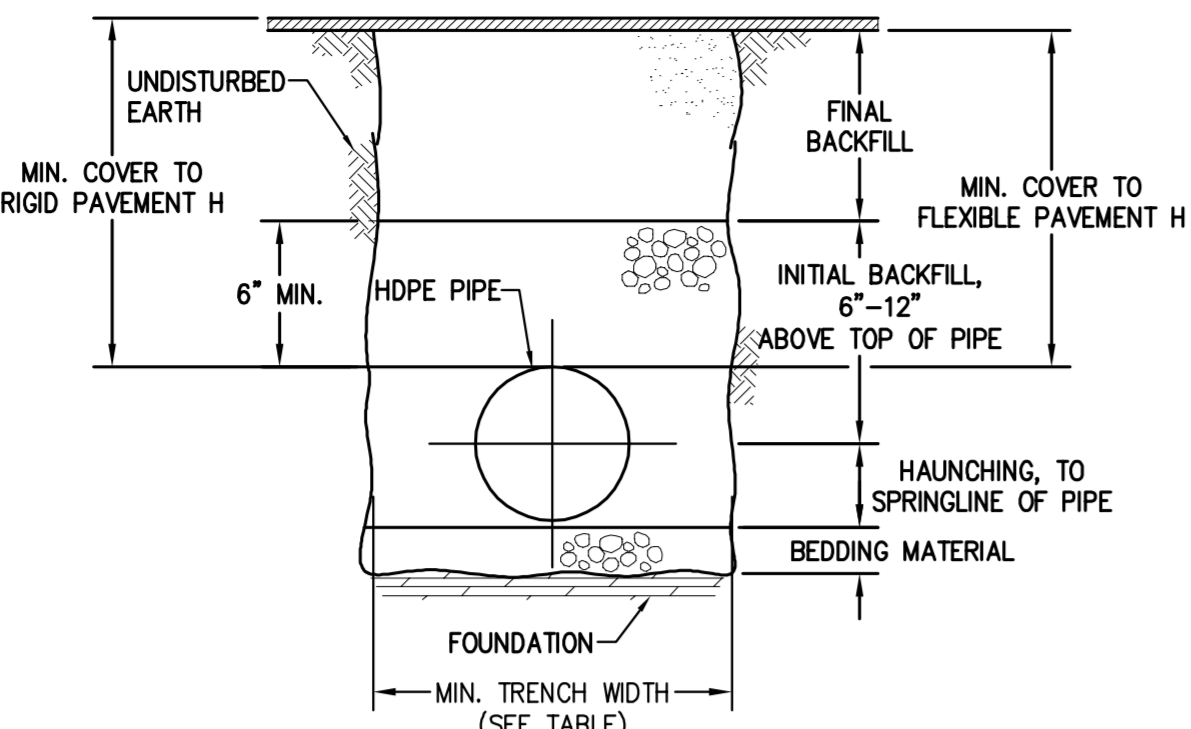
EROSION AND SEDIMENTATION CONTROL LEGEND

(CE)		CONSTRUCTION ENTRANCE (SPEC. 3.02)
(SF)		SILT FENCE (SPEC. 3.05)
(CIP)		CULVERT INLET PROTECTION (SPEC. 3.08-1 WITH STONE COMBINATION INSTEAD OF SILT FENCE)
(TO)		TOPSOILING (SPEC. 3.30)
(TS)		TEMPORARY SEEDING (SPEC. 3.31)
(PS)		PERMANENT SEEDING (SPEC. 3.32)
(TP)		TREE PROTECTION (SPEC. 3.38)
(DC)		DUST CONTROL (SPEC. 3.39)

NOTE:
SEE VIRGINIA EROSION & SEDIMENT CONTROL HANDBOOK FOR EROSION CONTROL SPECIFICATIONS (SPEC.) AND DETAILS.

IRONBOUND ROAD
ROUTE 615 - VAR. WIDTH R/W

B:\08\B\0705_Colonial Vet Clinic Vet Expansion\Drawings\01010001_Plan E&S.dwg, 01/12/2022 10:11:32 AM, RYAN STEPHANSON



NOTES:

- MATERIALS:** UNLESS OTHERWISE SPECIFIED ON THE PLANS OR HEREIN, CORRUGATED POLYETHYLENE PIPE SHALL CONFORM TO AASHTO M-294, LATEST EDITION, STANDARD SPECIFICATION FOR CORRUGATED POLYETHYLENE PIPE.
RESINS: CORRUGATED POLYETHYLENE PIPE SHALL BE MANUFACTURED FROM HIGH DENSITY POLYETHYLENE VIRGIN COMPOUNDS, AND SHALL CONFORM TO THE REQUIREMENTS OF ASTM D-3350 FOR THE CELL CLASSIFICATION 335400C.
COUPLING BANDS: EXCEPT AS OTHERWISE REQUIRED HEREIN, COUPLING BANDS AND OTHER HARDWARE FOR CORRUGATED POLYETHYLENE PIPE SHALL DEMONSTRATE THAT THEY MEET THE SOIL TIGHTNESS REQUIREMENTS OF AASHTO SECTION 26 "STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES."
COUPLING BANDS SHALL LAP EQUALLY ON EACH OF THE PIPES BEING CONNECTED TO FORM A TIGHTLY CLOSED JOINT AFTER INSTALLATION.
THE CORRUGATIONS IN THE BAND SHALL INDEX THE CORRUGATIONS IN THE PIPE ENDS TO ENGAGE AT LEAST TWO FULL CORRUGATIONS FROM THE END OF EACH PIPE.
WHEN INFILTRATION OR EXFILTRATION IS A CONCERN, THE COUPLING BANDS MAY BE REQUIRED TO HAVE GASKETS. THE GASKET MATERIAL SHALL BE CLOSED-CELL EXPANDED RUBBER OR NEOPRENE.
OTHER COUPLINGS MAY BE BELL & SPIGOT AND CONFORM TO THE REQUIREMENTS OF AASHTO M294.
DESIGNATION OF TYPE: THE PIPE SHALL BE THE FOLLOWING TYPE:
TYPE S: THIS PIPE WILL HAVE A FULL CIRCULAR CROSS-SECTION, WITH AN OUTER CORRUGATED PIPE WALL AND A SMOOTH INNER LINER.
- FOUNDATION:** WHERE THE TRENCH BOTTOM IS UNSTABLE, THE CONTRACTOR SHALL EXCAVATE TO A DEPTH REQUIRED BY THE ENGINEER AND REPLACE WITH SUITABLE MATERIAL IN ACCORDANCE WITH ASTM D2321, LATEST EDITION. AS AN ALTERNATIVE AND AT THE DISCRETION OF THE DESIGN ENGINEER, THE TRENCH BOTTOM MAY BE STABILIZED USING GEOTEXTILE MATERIAL.
- BEDDING:** SUITABLE MATERIAL SHALL BE CLASS I, II OR III IN ACCORDANCE WITH ASTM D2321, LATEST EDITION. THE CONTRACTOR SHALL PROVIDE DOCUMENTATION FOR MATERIAL SPECIFICATION TO ENGINEER. UNLESS OTHERWISE NOTED BY THE ENGINEER, MINIMUM BEDDING THICKNESS SHALL BE 4" FOR 4"-24"; 6" FOR 30"-60".
- HAUNCHING AND INITIAL BACKFILL:** SUITABLE MATERIAL SHALL BE CLASS I, II OR III IN THE PIPE ZONE EXTENDING NOT LESS THAN 6" ABOVE CROWN OF PIPE. THE CONTRACTOR SHALL PROVIDE DOCUMENTATION FOR MATERIAL SPECIFICATION TO ENGINEER. MATERIAL SHALL BE INSTALLED AS REQUIRED IN ASTM D2321, LATEST EDITION.
- MINIMUM TRENCH WIDTHS SHALL BE AS FOLLOWS:**

DIAMETER	MIN. TRENCH WIDTH	DIAMETER	MIN. TRENCH WIDTH	DIAMETER	MIN. TRENCH WIDTH
4"	21"	15"	34"	42"	72"
6"	23"	18"	39"	48"	80"
8"	26"	24"	48"	54"	88"
10"	28"	30"	56"	60"	96"
12"	30"	36"	64"		

THE TRENCH WIDTH MUST BE WIDE ENOUGH TO ACCOMMODATE COMPACTION EQUIPMENT.

- MINIMUM COVER:** MINIMUM COVER, H, IN NON-TRAFFIC APPLICATIONS (GRASS OR LANDSCAPE AREAS) IS 12" FROM THE TOP OF PIPE TO GROUND SURFACE. ADDITIONAL COVER MAY BE REQUIRED TO PREVENT FLOATATION. FOR TRAFFIC APPLICATIONS, MINIMUM COVER, H, IS MEASURED FROM THE TOP OF PIPE TO BOTTOM OF FLEXIBLE PAVEMENT OR TO TOP OF RIGID PAVEMENT.

PIPE DIAM.	MINIMUM RECOMMENDED COVER	
	H-25	HEAVY CONSTRUCTION (75T AXLE LOAD)*
12"-48"	12"	48"
54"-60"	24"	60"

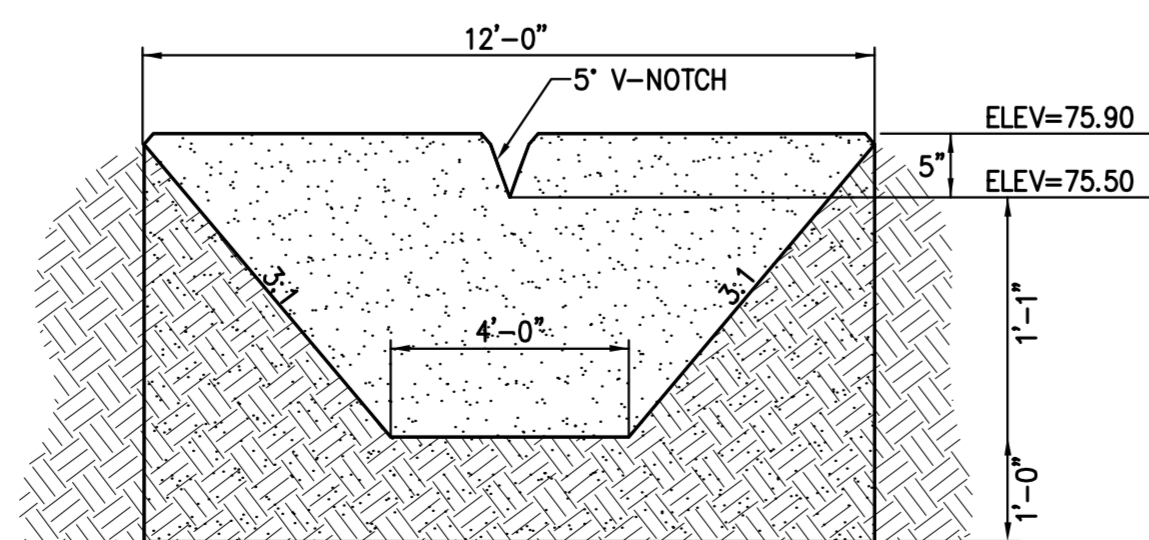
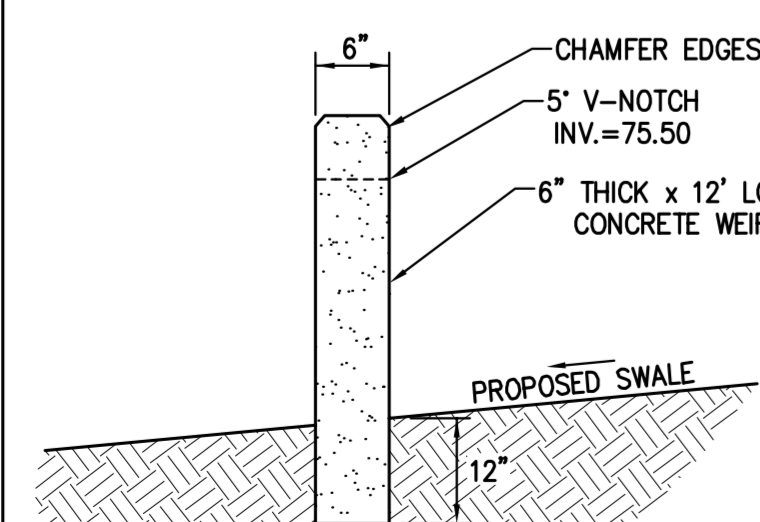
* VEHICLES IN EXCESS OF 75T MAY REQUIRE ADDITIONAL COVER

- CONNECTION TO CONCRETE STRUCTURES:** CONNECTIONS SHALL MEET SPECIFICATIONS ASTM C923.
- PERFORATED HDPE PIPE:** SHALL HAVE FIVE HOLES, ONE INCH IN DIAMETER EVENLY SPACED AND PLACED WITHIN AN ARC OF 130 DEGREES, THE CENTER HOLE BEING DIRECTLY OPPOSITE THE TOP MARKING. SPACING OF EACH SET OF FIVE HOLES SHALL BE AT 8 INCH INTERVALS ALONG THE TUBE. THE HOLES WILL BE DRILLED AND BE FREE OF ANY DEFECTS. THE PERFORATED PIPE SHALL BE INSTALLED SO THAT THE CENTER HOLE IS IN THE HORIZONTAL PLANE AND INTERFACES WITH THE GRADED GRAVEL.

HDPE PIPE INSTALLATION DETAIL

N.T.S.

2 YEAR



CONCRETE WEIR

N.T.S.

GENERAL

- METHOD OF PIPE BEDDING SHALL BE USED FOR ALL TYPES OF PIPE CULVERTS WITHIN THE APPLICABLE HEIGHT OF COVER RANGE NOTED IN THE STANDARD P.C.I. TABLES UNLESS OTHERWISE NOTED ON THE PLANS.
- H = HEIGHT OF COVER MEASURED FROM TOP OF CULVERT TO FINISHED GRADE.
- D = EXCAVATION DEPTH AS SHOWN ON PLANS OR TO FIRM BEARING SOIL.

CIRCULAR PIPE

- D = OUTSIDE DIAMETER OF PIPE.
- d = INSIDE DIAMETER OF PIPE.
- X = WIDTH OF CLASS I BACKFILL MATERIAL BEYOND THE EXTREMITY OF THE PIPE.
X = 12" WHERE d IS LESS THAN 36"
X = 18" WHERE d IS 36" AND GREATER.
- WHERE DIRECTED BY THE ENGINEER, BEDDING MATERIAL MAY BE ELIMINATED FOR NORMAL EARTH FOUNDATIONS UNDER ROUTINE ENTRANCE PIPE EXCEPT PLASTIC PIPE 36" AND LESS IN DIAMETER WITH HEIGHT OF COVER 15' OR LESS.
- REGULAR BACKFILL MATERIAL MAY BE USED IN LIEU OF CLASS I BACKFILL MATERIAL FOR ALL FOUNDATION TYPES FOR ROUTINE ENTRANCE PIPE WHERE S₂ IS 35' OR LESS AND HEIGHT OF COVER 15' OR LESS.
- REGULAR BACKFILL MATERIAL MAY BE USED IN LIEU OF CLASS I BACKFILL MATERIAL FOR ALL FOUNDATION TYPES FOR ROUTINE ENTRANCE PIPE WHERE S₂ IS 35' OR LESS AND HEIGHT OF COVER 15' OR LESS.

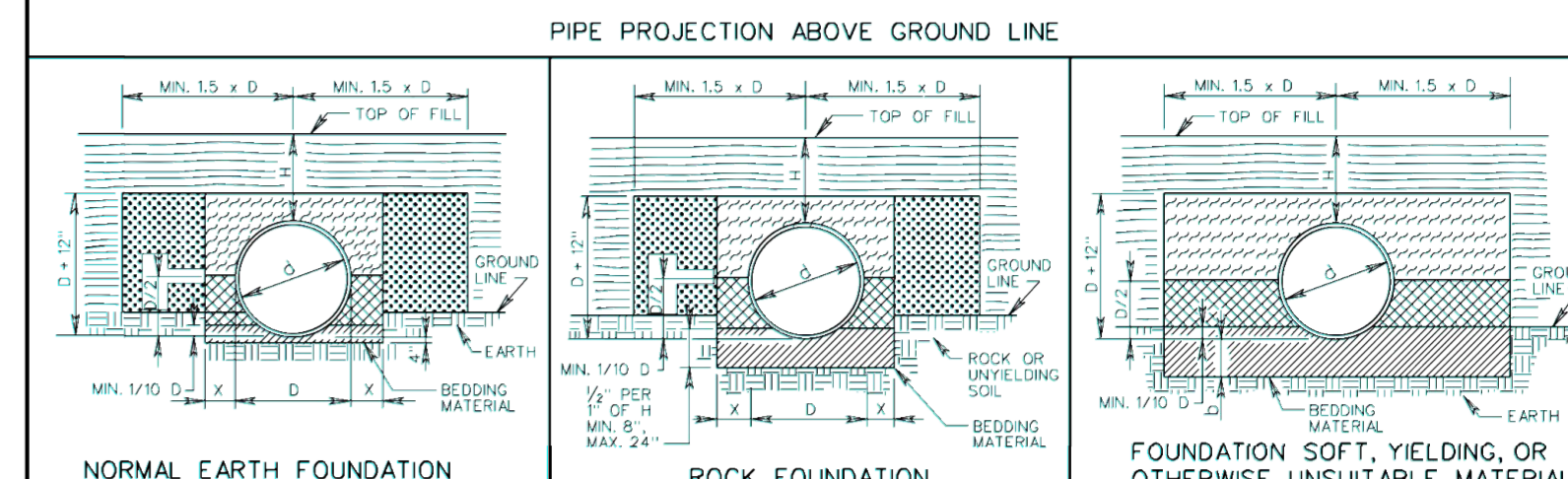
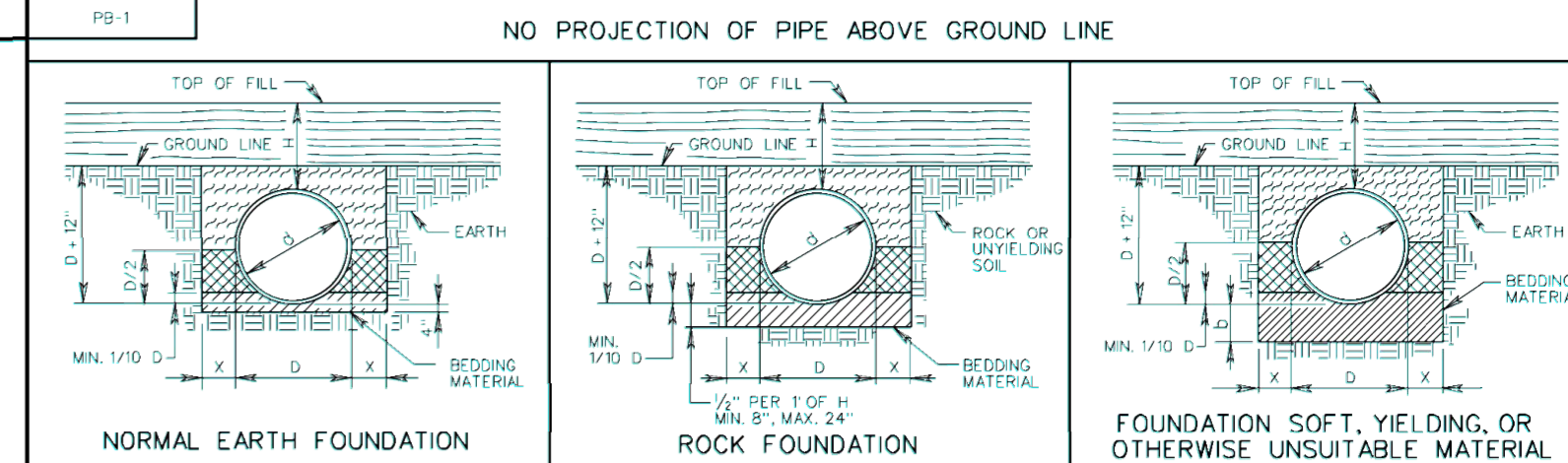
ELLIPTICAL PIPE

- S₁ = OUTSIDE SPAN DIMENSION OF PIPE.
- S₂ = INSIDE SPAN DIMENSION OF PIPE.
- R = OUTSIDE RISE DIMENSION OF PIPE.
- X = WIDTH OF CLASS I BACKFILL MATERIAL BEYOND THE EXTREMITY OF THE PIPE.
X = 12" WHERE S₂ IS LESS THAN 36"
X = 18" WHERE S₂ IS 36" AND GREATER.
- WHERE DIRECTED BY THE ENGINEER, BEDDING MATERIAL MAY BE ELIMINATED FOR NORMAL EARTH FOUNDATIONS UNDER ROUTINE ENTRANCE PIPE WHERE S₂ IS 35' OR LESS AND HEIGHT OF COVER 15' OR LESS.
- REGULAR BACKFILL MATERIAL MAY BE USED IN LIEU OF CLASS I BACKFILL MATERIAL FOR ALL FOUNDATION TYPES FOR ROUTINE ENTRANCE PIPE WHERE S₂ IS 35' OR LESS AND HEIGHT OF COVER 15' OR LESS.

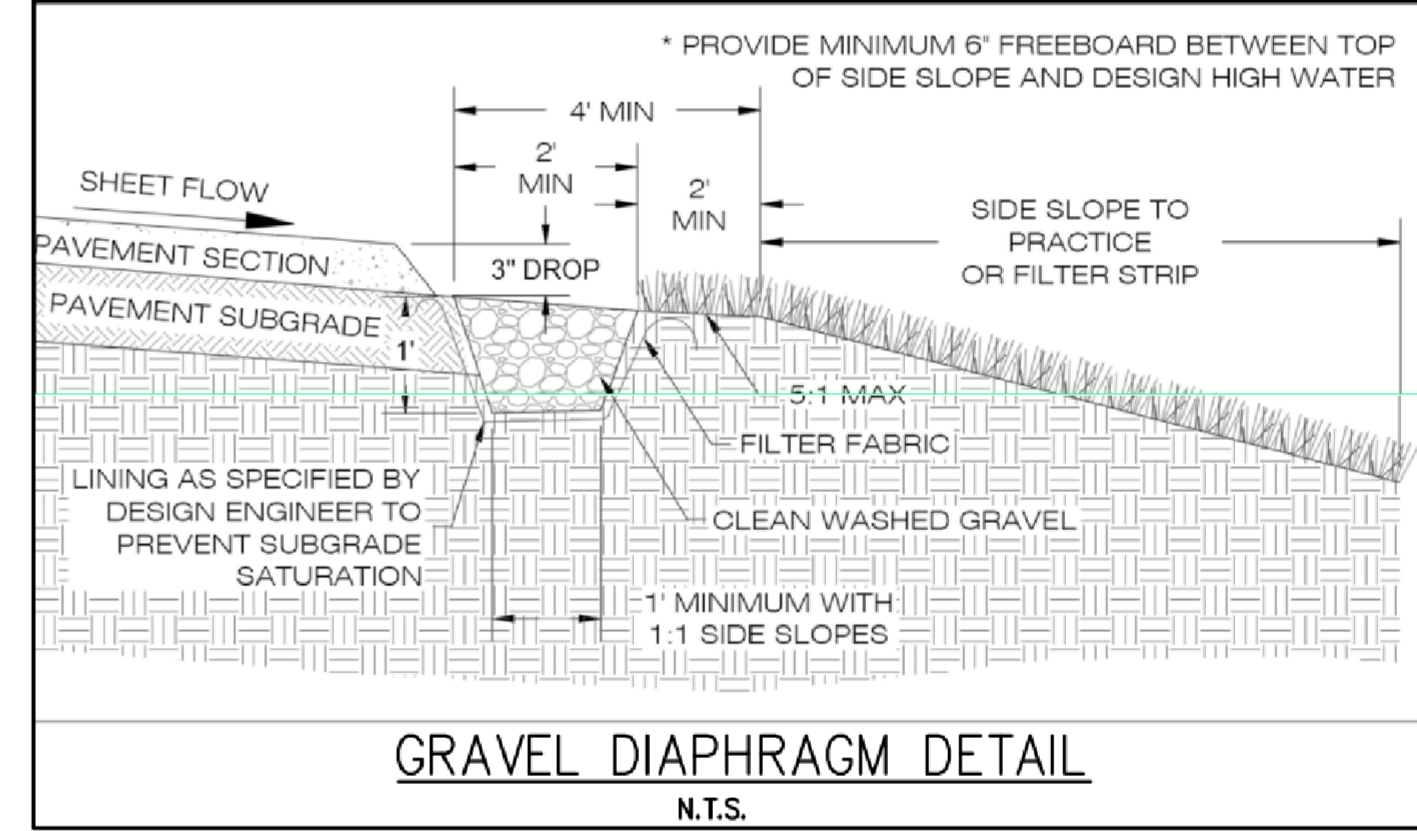
PIPE ARCH

- S = SPAN DIMENSION OF PIPE.
- R = RISE DIMENSION OF PIPE.
- D = SEE P.C.I. TABLE FOR APPLICABLE PIPE MATERIAL.
- X = WIDTH OF CLASS I BACKFILL MATERIAL BEYOND THE EXTREMITY OF THE PIPE.
X = 12" WHERE S₂ IS LESS THAN 36"
X = 18" WHERE S₂ IS 36" AND GREATER.
- WHERE DIRECTED BY THE ENGINEER, BEDDING MATERIAL MAY BE ELIMINATED FOR NORMAL EARTH FOUNDATIONS UNDER ROUTINE ENTRANCE PIPE WHERE S₂ IS 35' OR LESS AND HEIGHT OF COVER 15' OR LESS.
- REGULAR BACKFILL MATERIAL MAY BE USED IN LIEU OF CLASS I BACKFILL MATERIAL FOR ALL FOUNDATION TYPES FOR ROUTINE ENTRANCE PIPE WHERE S₂ IS 35' OR LESS AND HEIGHT OF COVER 15' OR LESS.

SPECIFICATION REFERENCE 302 303	INSTALLATION OF PIPE CULVERTS & STORM SEWERS GENERAL NOTES VIRGINIA DEPARTMENT OF TRANSPORTATION	ROAD AND BRIDGE STANDARDS REVISION DATE SHEET 1 OF 1 9/7/09	VDOT ROAD AND BRIDGE STANDARDS SHEET 1 OF 4 REVISION DATE 9/7/12	INSTALL. OF PIPE CULVERTS AND STORM SEWERS CIRC. PIPE BEDDING AND BACKFILL - METHOD "A" VIRGINIA DEPARTMENT OF TRANSPORTATION	SPECIFICATION REFERENCE 302 303
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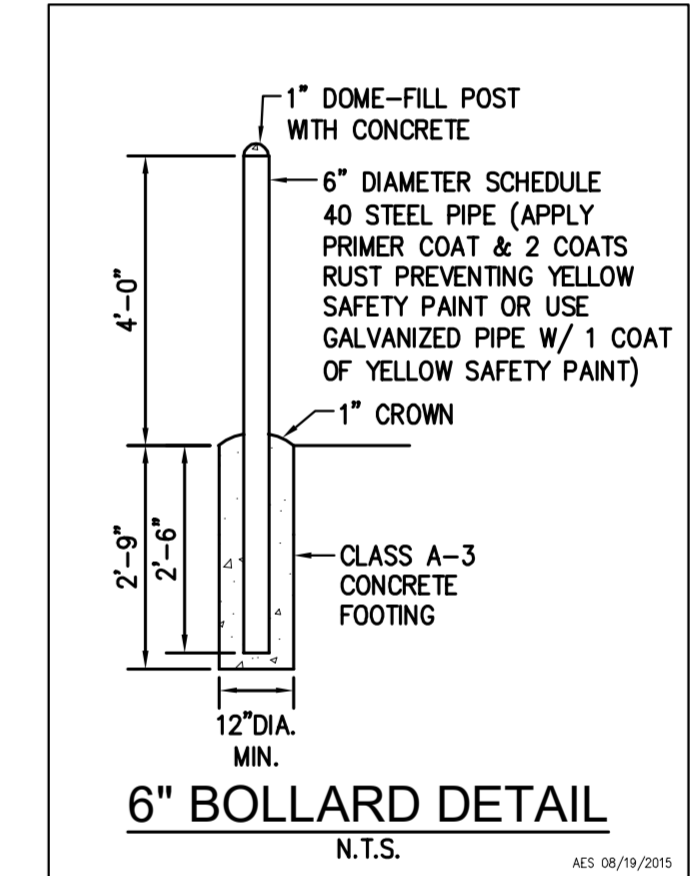


- LEGEND:**
- BEDDING MATERIAL IN ACCORDANCE WITH SECTION 302 OF THE ROAD AND BRIDGE SPECIFICATIONS.
 - CLASS I BACKFILL MATERIAL IN ACCORDANCE WITH SECTION 302 OF THE ROAD AND BRIDGE SPECIFICATIONS.
 - FOR PLASTIC PIPE CLASS I BACKFILL MATERIAL IN ACCORDANCE WITH SECTION 302 OF THE ROAD AND BRIDGE SPECIFICATIONS.
 - FOR ALL OTHER PIPE REGULAR BACKFILL MATERIAL IN ACCORDANCE WITH SECTION 302 OF THE ROAD AND BRIDGE SPECIFICATIONS.
 - EMBANKMENT
 - REGULAR BACKFILL MATERIAL IN ACCORDANCE WITH SECTION 302 OF THE ROAD AND BRIDGE SPECIFICATIONS.
 - CRUSHED CLASS II CONFORMING TO THE SIZE REQUIREMENTS FOR CURBSIDE RUN AGGREGATE SIZE 35 AND 60 MAY BE USED IN PLACE OF CLASS I BACKFILL.
- NOTES:**
- FOR GENERAL NOTES ON PIPE BEDDING, SEE INSTALLATION OF PIPE CULVERTS AND STORM SEWERS GENERAL NOTES ON SHEET 107.023.



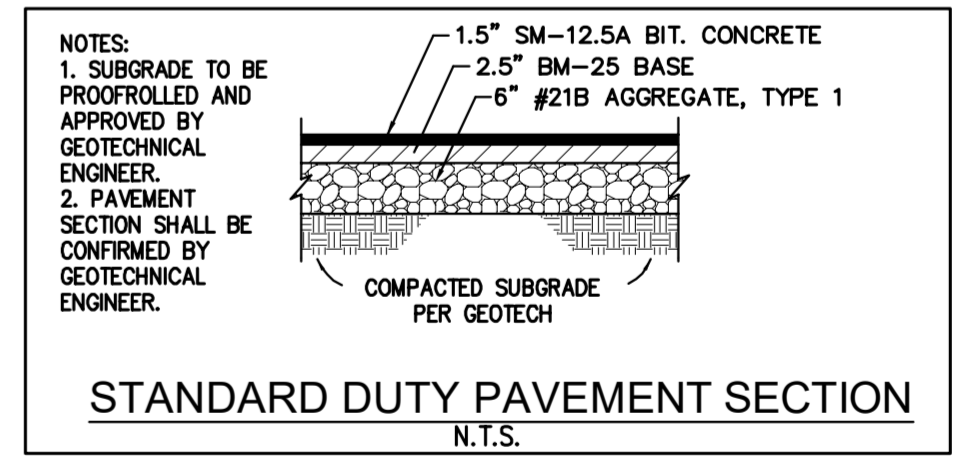
GRAVEL DIAPHRAGM DETAIL

N.T.S.



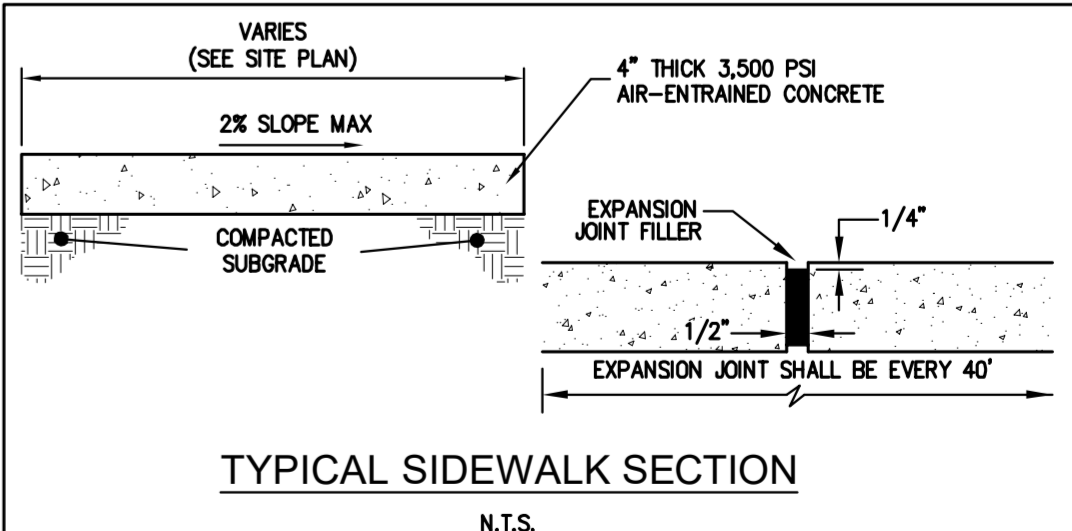
6" BOLLARD DETAIL

N.T.S.



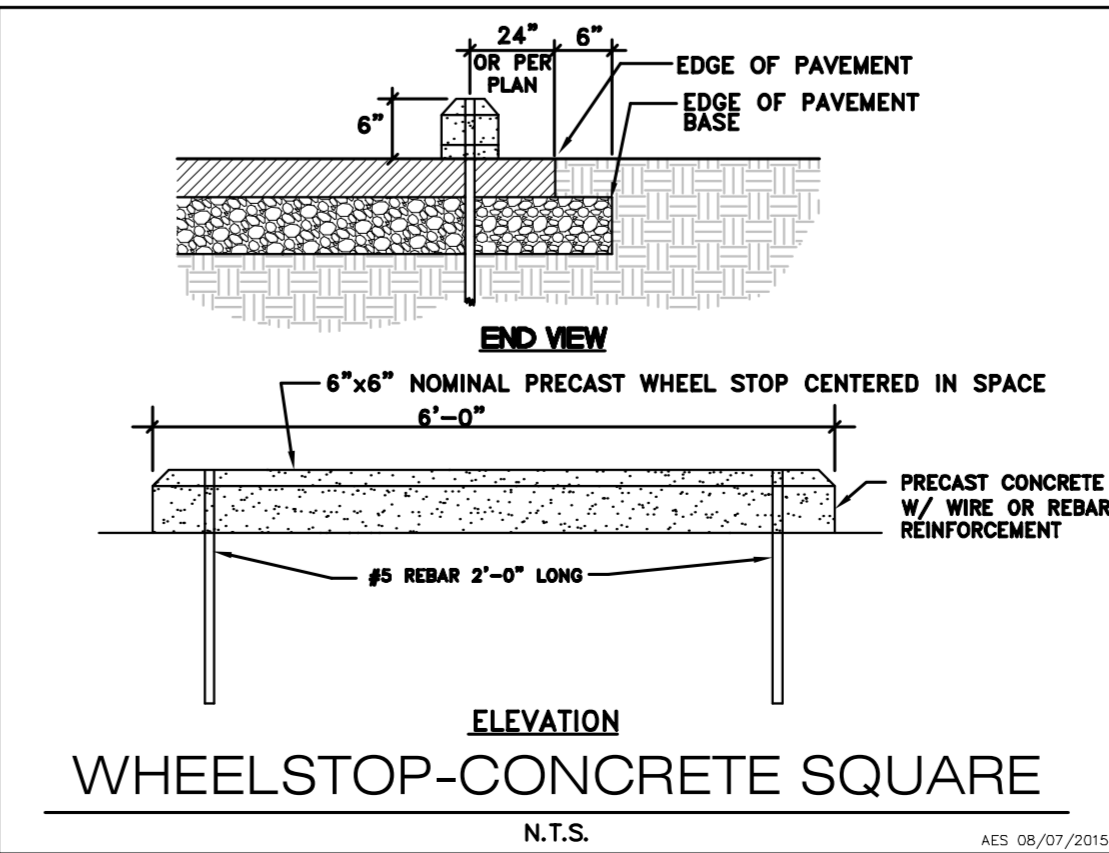
STANDARD DUTY PAVEMENT SECTION

N.T.S.



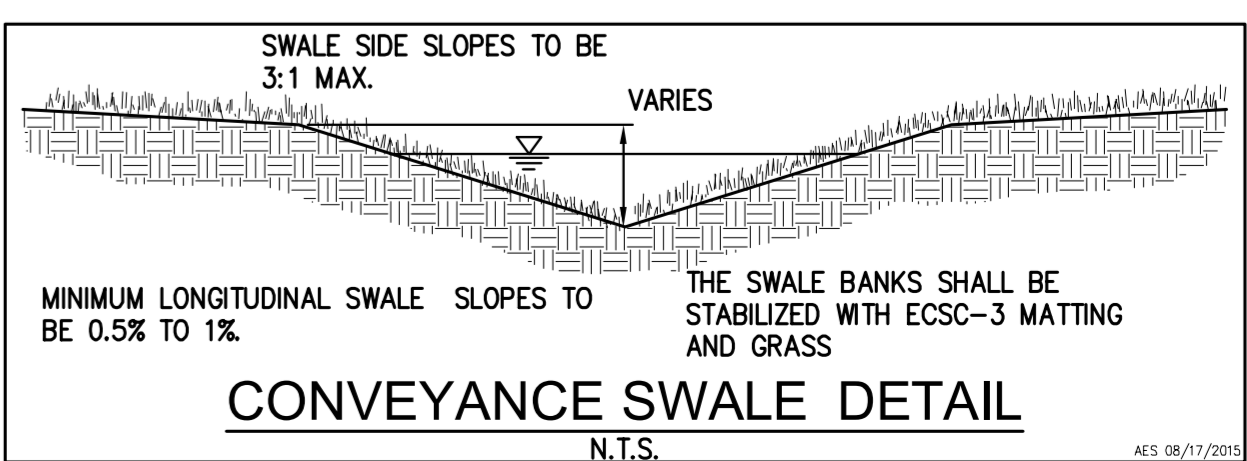
TYPICAL SIDEWALK SECTION

N.T.S.



WHEELSTOP-CONCRETE SQUARE

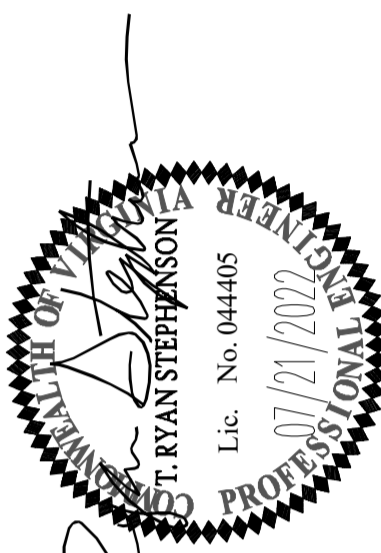
N.T.S.



CONVEYANCE SWALE DETAIL

N.T.S.

Rev.	Date	Description
1	07/21/2022	REVISED PER 1ST SUBMITTAL ACC COMMENTS



THIS PLAN HAS NOT RECEIVED FINAL APPROVAL AND IS NOT APPROVED FOR CONSTRUCTION.

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Phone: (757) 255-0640
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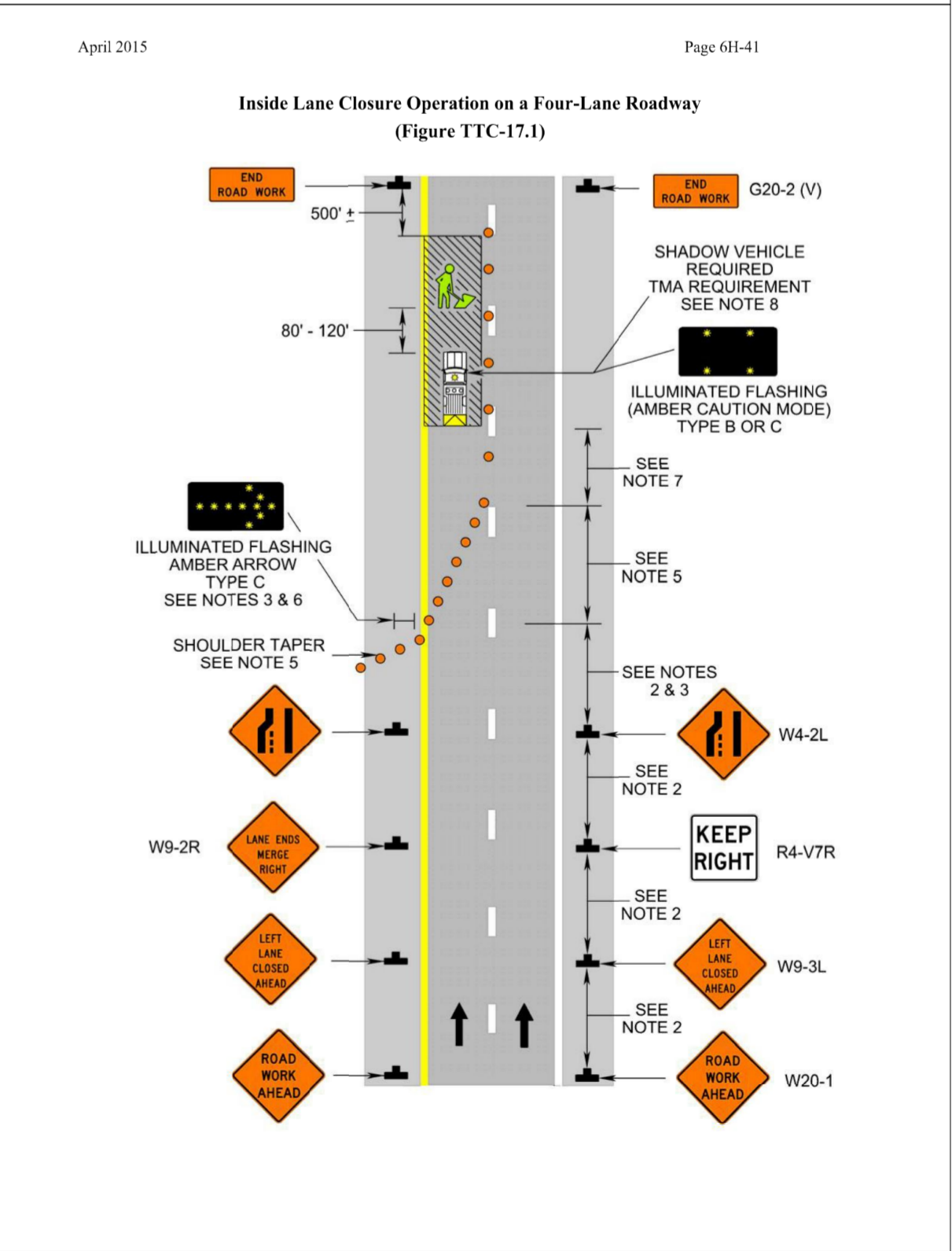
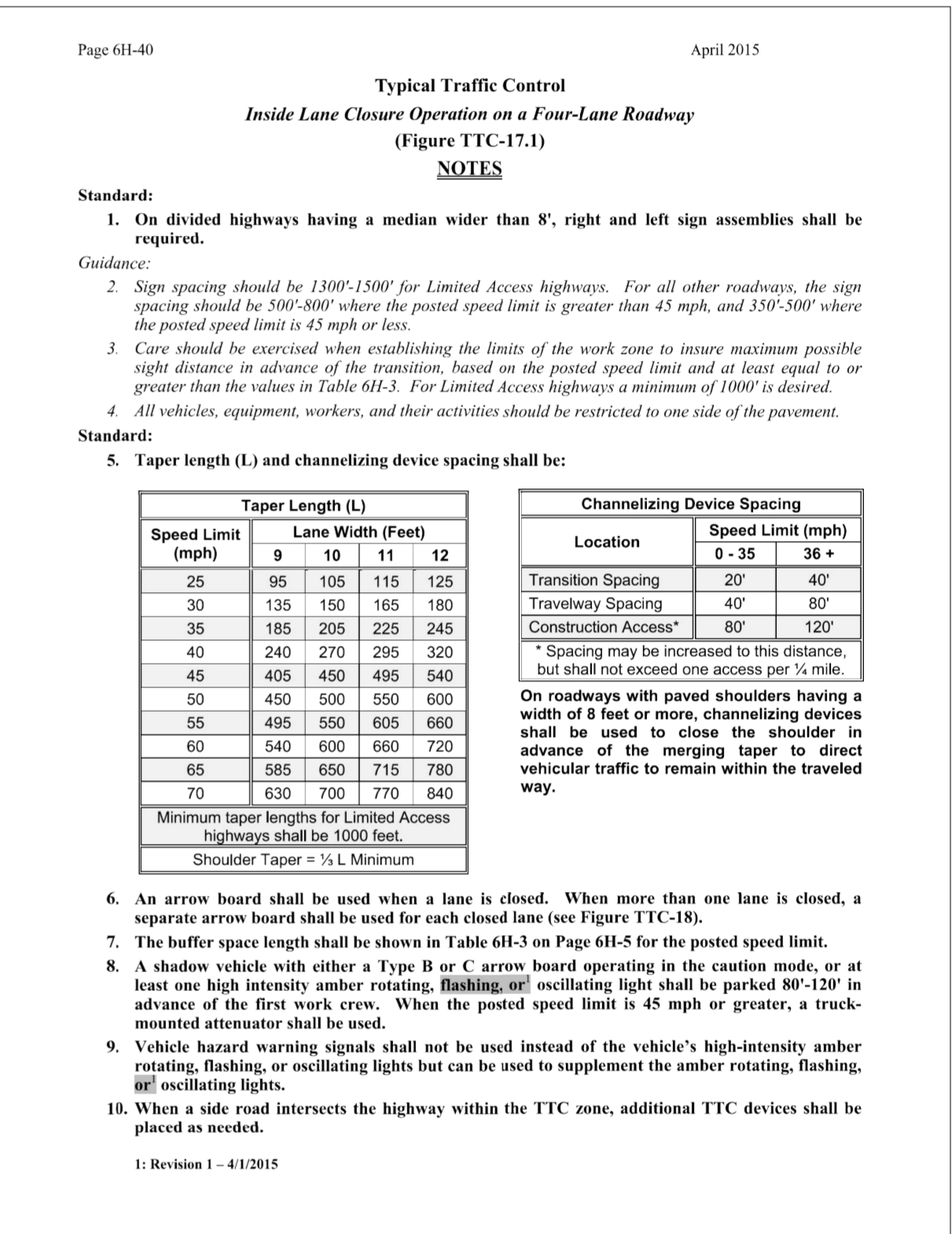
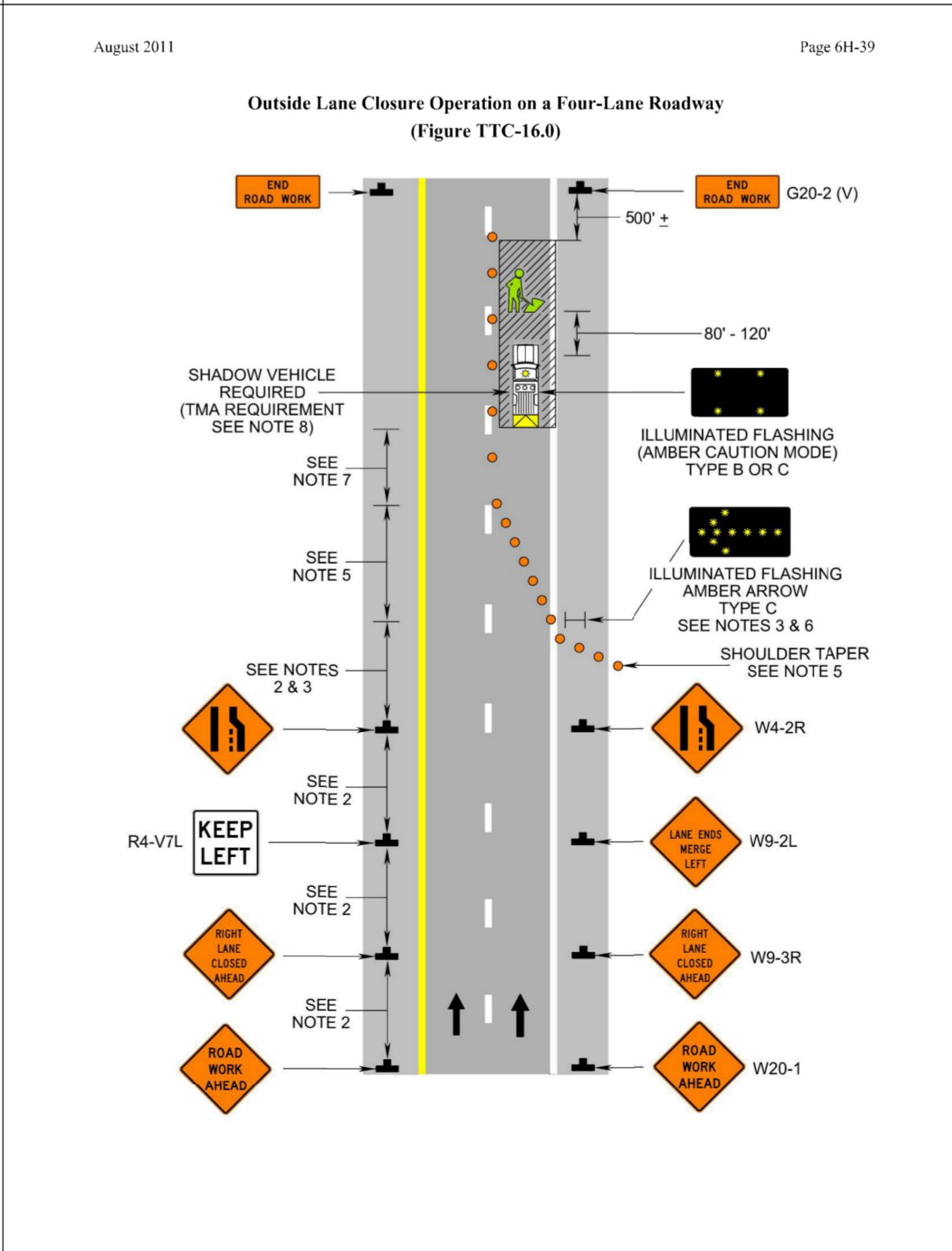
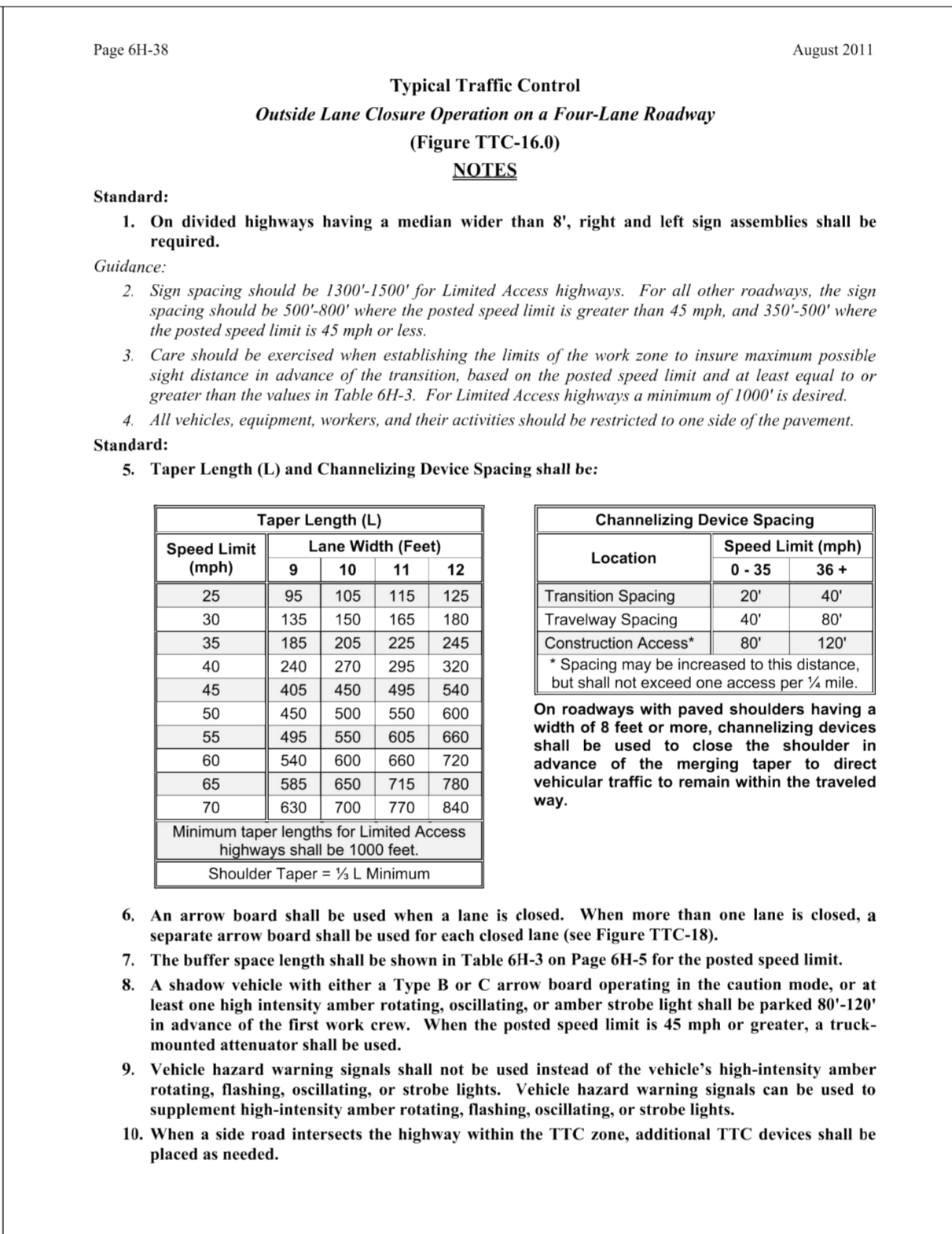
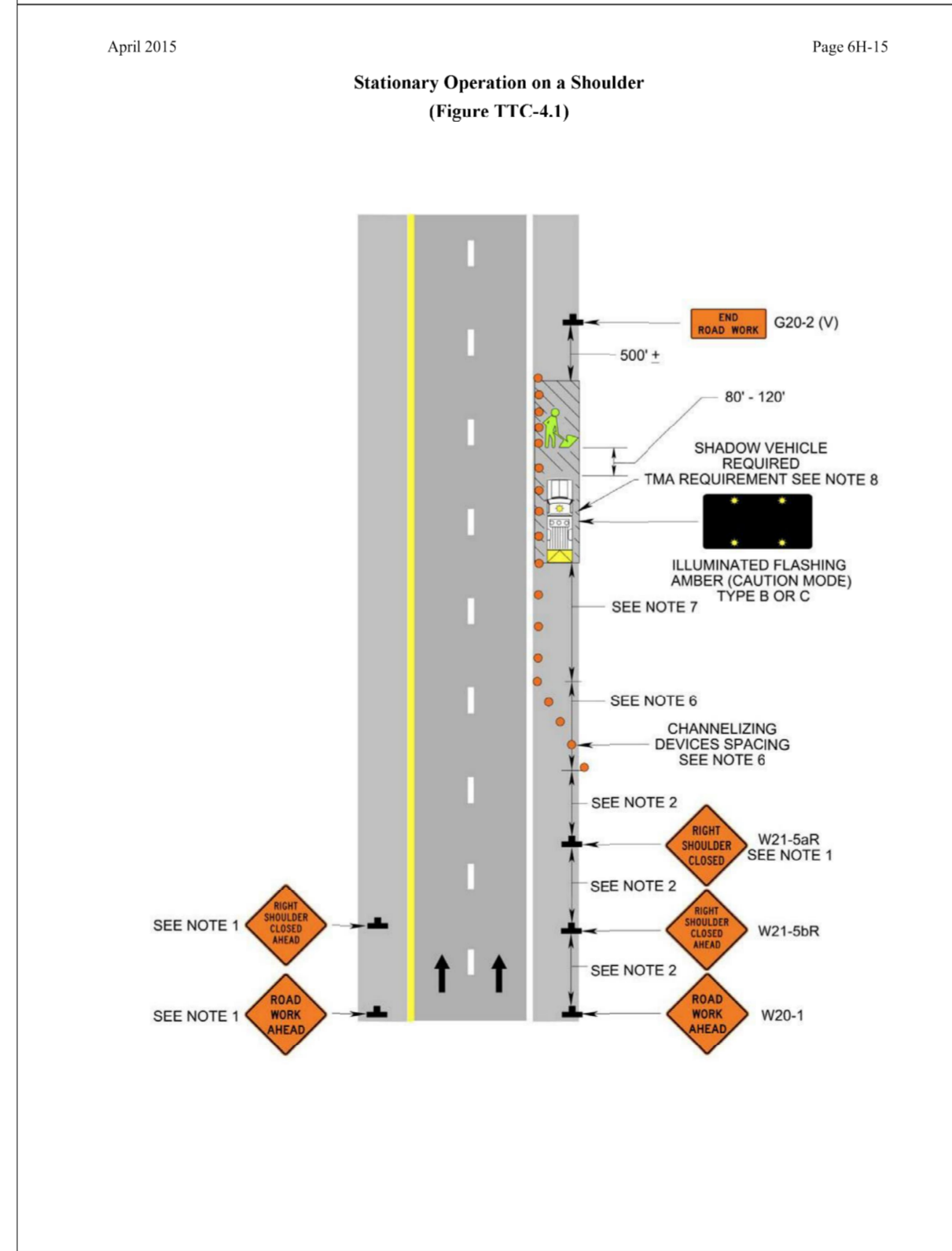
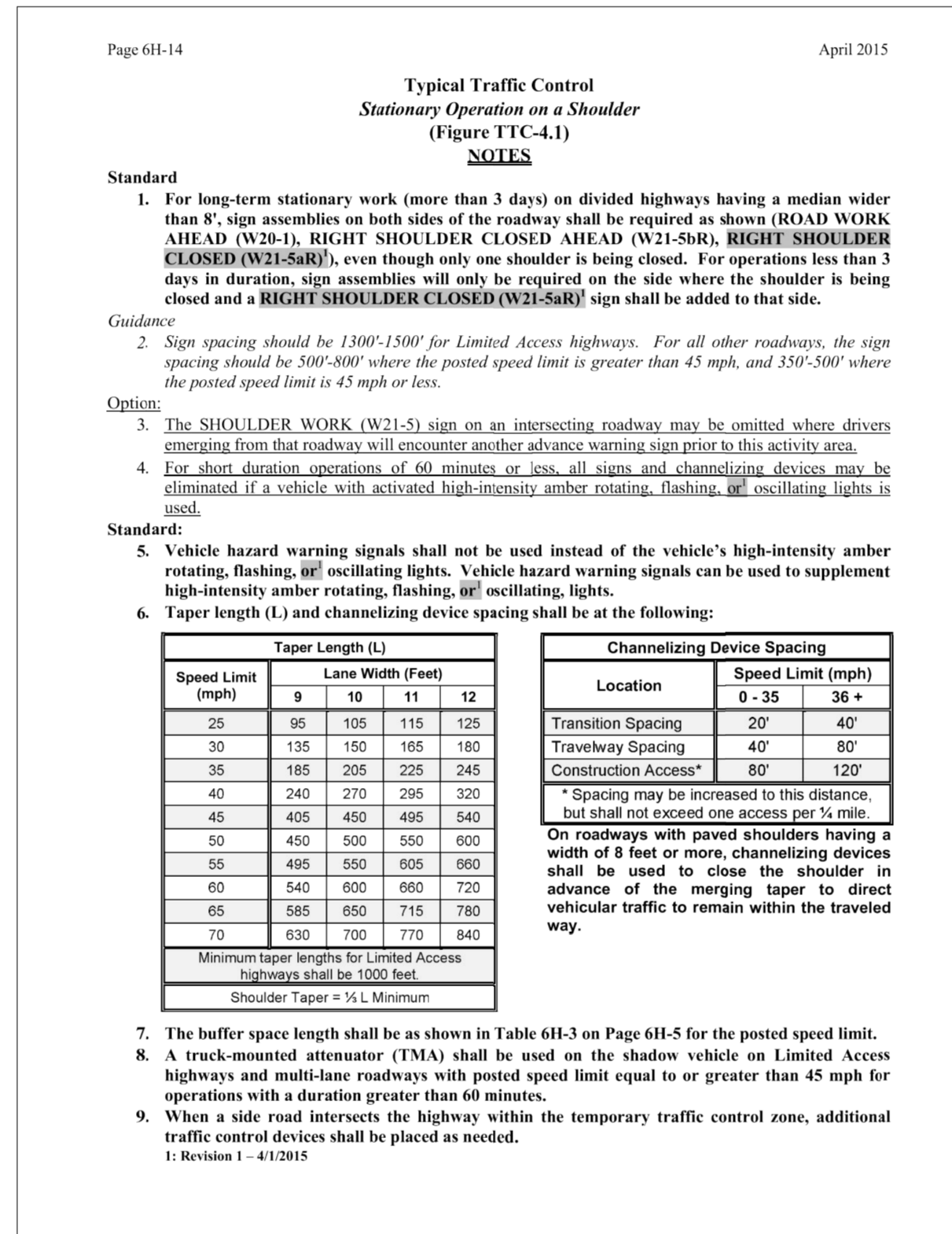
SITE PLAN FOR
COLONIAL VETERINARY CLINIC PARKING EXPANSION

Project Contacts: TRS
Project Number: W10705-00
Scale: AS NOTED
Date: 01/12/2022

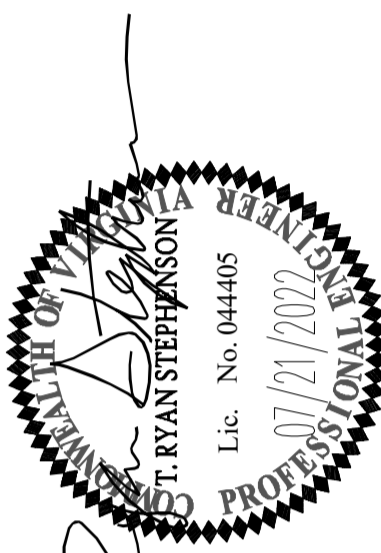
Sheet Title:
SITE NOTES AND DETAILS

Sheet Number
C6.1

- 1.0 GENERAL INFORMATION
- 1.1 THESE PLANS WERE PREPARED IN ACCORDANCE WITH THE REQUIREMENTS OF (SELECT ONE):
 VDOT SUBDIVISION STREET REQUIREMENTS (SSR)
 VDOT SUBDIVISION STREET ACCEPTANCE REQUIREMENTS (SSAR)
 N/A
- 1.2 VDOT APPROVED EXCEPTIONS (SELECT ONE):
 ACCESS MANAGEMENT DATE OF APPROVAL _____
 SSAR DATE OF APPROVAL _____
 OTHER A-A: _____ DATE OF APPROVAL _____
 N/A
- 1.3 TRIP GENERATION DATA:
 USE: _____ PARKING LOT _____
 ITE CODE: _____
 TOTAL DAILY TRIPS: _____ VPD
 AM PEAK HOUR TRIPS: _____ VPH
 PM PEAK HOUR TRIPS: _____ VPH
- 1.4 A LAND USE PERMIT MUST BE OBTAINED FROM VDOT BEFORE ANY CONSTRUCTION IS STARTED WITHIN STATE MAINTAINED RIGHT OF WAY LIMITS. CONTACT THE VDOT DISTRICT OFFICE AT (757) 925-3686 FOR THE PERMIT FEE AND BOND AMOUNT. ALL LAND USE PERMIT APPLICATIONS MUST HAVE ONE (1) SET OF APPROVED PLANS, A COPY OF THE PLAN APPROVAL LETTER, A CHECK FOR THE PROCESSING FEE MADE PAYABLE TO TREASURER OF VIRGINIA, AND SURETY OR BOND IN THE REQUIRED AMOUNT.
- 1.5 THE VDOT DISTRICT OFFICE IS TO RECEIVE WRITTEN NOTIFICATION 48 HOURS PRIOR TO THE START OF ANY WORK WITHIN STATE MAINTAINED RIGHT OF WAY. A PRE-CONSTRUCTION MEETING SHOULD BE HELD AT LEAST ONE WEEK PRIOR TO ANY LAND DISTURBANCE OR WHEN CONSTRUCTION ACTIVITIES WILL AFFECT TRAFFIC OPERATIONS OF ANY ARTERIAL CORRIDOR.
- 1.6 ALL CONSTRUCTION METHODS AND MATERIALS WITHIN STATE MAINTAINED RIGHT OF WAY SHALL COMPLY WITH THE CURRENT STANDARDS AND SPECIFICATIONS OF THE VIRGINIA DEPARTMENT OF TRANSPORTATION.
- 1.7 THE CONTRACTOR SHALL HAVE AVAILABLE A COPY OF THE LAND USE PERMIT(S), FINAL APPROVED PLANS, ANY APPROVED REVISIONS, AND A COPY OF THE APPROVAL LETTER ON SITE.
- 1.8 ANY ERRORS, CONFLICTS, OR DISCREPANCIES FOUND ON THE APPROVED PLANS SHALL BE REPORTED TO THE DEVELOPER'S ENGINEER. THE VDOT DISTRICT OFFICE SHOULD BE NOTIFIED FOR RESOLUTION BEFORE PROCEEDING FURTHER WITH THE WORK IF THE STATE MAINTAINED RIGHT OF WAY IS AFFECTED.
- 1.9 THE DEVELOPER IS RESPONSIBLE FOR ANY DAMAGE TO EXISTING ROADS, UTILITIES, AND ANY OTHER INSTALLATIONS ALREADY IN PLACE WHICH OCCUR AS A RESULT OF PROJECT CONSTRUCTION WITHIN OR CONTIGUOUS TO STATE RIGHT OF WAY LIMITS.
- 1.10 THE COMMONWEALTH TRANSPORTATION BOARD MEMBERS OF THE BOARD, THE COMMONWEALTH AND ALL COMMONWEALTH EMPLOYEES, AGENTS, AND OFFICES, SHALL BE ABSOLVED FROM ALL RESPONSIBILITIES, DAMAGES AND LIABILITIES AS A RESULT OF WORK ARISING FROM THE EXERCISE OF THE PRIVILEGES GRANTED BY PLAN AND/OR PERMIT APPROVAL.
- 1.11 MATERIALS SHALL NOT BE STOCKPILED WITHIN STATE MAINTAINED RIGHT OF WAY OR EASEMENTS.
- 1.12 PRIVATE ENTRANCES LOCATED ON STREETS WITH CURB AND GUTTER (CG-6, CG-7) REQUIRE INSTALLATION OF CURB APRONS (CG-9B, CG-9D). PRIVATE ENTRANCES LOCATED ON STREETS WITH ROLLTOP CURB SHALL BE INSTALLED IN ACCORDANCE WITH THE DETAIL ON PAGE B(1)-29 OF THE VDOT ROAD DESIGN MANUAL. SIDEWALK SHALL BE 7" THICK AT DRIVEWAY CROSSINGS AND ADJACENT TO ROLLTOP CURB. INSTALLATION OF PIPE CULVERTS AND STORM SEWERS SHALL CONFORM TO THE CURRENT VDOT STANDARD PB-1. PIPE CULVERTS SHALL HAVE A MINIMUM OF 4" OF COVER UNDER CONCRETE GUTTERS, OR ADJUSTMENT SHALL BE REQUIRED TO CONCRETE GUTTER GRADES IN THE FIELD TO REACH THIS COVER OR AS DIRECTED BY THE VDOT ENGINEER. PRIVATE ENTRANCES LOCATED ON STREETS WITH ROADSIDE DITCHES REQUIRE 9" OF COVER.
- 2.0 DRAINAGE & EROSION CONTROL
- 2.1 VDOT SHALL NOT BE RESPONSIBLE FOR THE MAINTENANCE OF ANY STORMWATER MANAGEMENT FACILITY OR OUTFALL STRUCTURE LOCATED OUTSIDE OF STATE MAINTAINED RIGHT OF WAY LIMITS AND SHALL BE ABSOLVED FROM ALL RESPONSIBILITIES, DAMAGES AND LIABILITIES AS A RESULT OF SUCH.
- 2.2 ALL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR IN ACCORDANCE WITH THE VIRGINIA EROSION & SEDIMENT CONTROL HANDBOOK AND VIRGINIA STORMWATER MANAGEMENT PROGRAM. AN INDIVIDUAL CERTIFIED BY THE VIRGINIA DEPARTMENT OF ENVIRONMENTAL QUALITY HOLDING A RESPONSIBLE LAND DISTURBER CERTIFICATION IS TO BE IN CHARGE OF THE LAND DISTURBING ACTIVITY AND FOR CONTINUED MAINTENANCE OF THE EROSION AND SEDIMENT CONTROL ON THE WORK SITE AT ALL TIMES.
- 2.3 TEMPORARY DRAINAGE MEASURES SHALL BE INSTALLED DURING CONSTRUCTION TO PREVENT PONDING AND DIVERT SEDIMENT LADEN WATER.
- 2.4 ALL NECESSARY PRECAUTIONS SHALL BE TAKEN TO ENSURE AGAINST SILTATION OF ADJACENT PROPERTIES, DITCHES, STREAMS, AND CONSTRUCTION VEHICLES TRACKING SILT ONTO ADJACENT ROADWAYS. ADDITIONAL DITCH LININGS OR SILTATION AND EROSION CONTROL MEASURES SHALL BE PROVIDED AS DETERMINED NECESSARY BY VDOT AND/OR THE COUNTY/TOWN DURING FIELD REVIEW. ALL COSTS SHALL BE ASSUMED BY THE DEVELOPER.
- 2.5 ALL DISTURBED AREAS SHALL BE STABILIZED IMMEDIATELY UPON THE END OF EACH DAY'S WORK AND RESEDED IN ACCORDANCE WITH VDOT ROAD AND BRIDGE SPECIFICATIONS.
- 2.6 STRUCTURES EXCEEDING 4' IN DEPTH REQUIRE INSTALLATION OF STEPS IN ACCORDANCE WITH STANDARD ST-1. STRUCTURES EXCEEDING 12' IN DEPTH REQUIRE SAFETY SLABS IN ACCORDANCE WITH STANDARD SL-1. IS-1 IS REQUIRED ON ALL DRAINAGE STRUCTURES.
- 2.7 ROOF DRAIN CONNECTIONS ARE NOT PERMITTED IN STRUCTURES WITHIN THE VDOT RIGHT OF WAY.
- 2.8 FOR SAFETY CONCERNS IN RESIDENTIAL SUBDIVISIONS, THE MINIMUM LENGTH OF AN ENTRANCE CULVERT SHOULD BE THE ENTRANCE WIDTH PLUS 8'. FOR EXAMPLE, A 16' WIDE ENTRANCE WOULD REQUIRE A 24' CULVERT. THE LENGTH MAY BE LESSEMED WHEN END WALLS ARE USED.
- 3.0 MATERIALS
- 3.1 INITIAL PAVEMENT DESIGN PROVIDED IS BASED ON A(N) ASSUMED VALUE. FINAL PAVEMENT DESIGN SHALL BE SUBMITTED IN ACCORDANCE WITH THE PAVEMENT DESIGN GUIDE FOR SUBDIVISION AND SECONDARY ROADS IN VIRGINIA. A COPY OF THE CBR REPORT SHALL BE PROVIDED PRIOR TO CONSTRUCTION. FOR PRIMARY ROADS AND INTERSTATE HIGHWAYS WHERE TRUCK TRAFFIC EXCEEDS 5% PAVEMENT DESIGN SHALL BE PROVIDED IN ACCORDANCE WITH AASHTO GUIDELINES. TYPICAL PAVEMENT SECTIONS SHALL DEPICT THE PROPOSED PAVEMENT STRUCTURE INCLUDING THE TOP 6" OF THE SUBGRADE IMMEDIATELY UNDER THE PAVEMENT STRUCTURE COMPACTED TO 100% OF THE THEORETICAL MAXIMUM DRY DENSITY.
- 3.2 WHEN UNSUITABLE MATERIALS ARE ENCOUNTERED DURING CONSTRUCTION, SUCH MATERIALS SHALL BE UNDERCUT AND BACKFILLED WITH VDOT SELECT MATERIAL TYPE II MINIMUM CBR-20 ACCORDING TO VDOT SPECIFICATIONS. THE SOIL LAB TEST REPORTS OF THE PROPOSED SELECT MATERIAL SHALL BE INCLUDED IN THE GEOTECHNICAL SOILS REPORT.
- 3.3 ASPHALT PAVEMENT WIDENING SHALL CONFORM TO THE CURRENT VDOT STANDARD WP-2.
- 3.4 THE DEPARTMENT USUALLY PROHIBITS THE OPEN-CUTTING OF HARD-SURFACED ROADS EXCEPT IN EXTENUATING CIRCUMSTANCES. THEREFORE, ALL UNDERGROUND UTILITIES WITHIN STATE MAINTAINED RIGHT OF WAY, AS DETERMINED NECESSARY BY GOOD ENGINEERING PRACTICE TO SERVE THE COMPLETE DEVELOPMENT OF ADJACENT PROPERTIES, SHALL BE INSTALLED DURING THE STREETS INITIAL CONSTRUCTION AND PRIOR TO THE APPLICATION OF ITS FINAL PAVEMENT SURFACE COURSE. THIS SHALL INCLUDE EXTENSIONS OF ALL NECESSARY CROSS-STREET CONNECTIONS OR SERVICE LINES TO AN APPROPRIATE LOCATION BEYOND THE PAVEMENT AND PREFERABLY THE RIGHT OF WAY LINE. IN THE EVENT IT IS NECESSARY TO OPEN THE STREET TEMPORARILY TO WORK ON UTILITIES AFTER THE SURFACE HAS BEEN PLACED, ADDITIONAL COMPACTION TESTS AND PAVING AS NECESSARY TO RESTORE THE INTEGRITY AND APPEARANCE OF THE ROADWAY MAY BE REQUIRED.
- 4.0 RIGHT OF WAY & UTILITIES
- 4.1 ALL FIXED OBJECTS SUCH AS UTILITY CABINETS, PEDESTALS, AND STREETLIGHTS SHALL BE LOCATED IN ACCORDANCE WITH CLEAR ZONE REQUIREMENTS, AS NOTED IN THE ROAD DESIGN MANUAL OR AASHTO ROADSIDE DESIGN GUIDE. THERE SHALL NOT BE ANY CABINETS, PEDESTALS, OR FIRE HYDRANTS LOCATED ON THE SHOULDER.
- 4.2 FLOWERS, SHRUBS, AND TREES SHALL NOT BE PLACED WITHIN STATE MAINTAINED RIGHT OF WAY LIMITS WITHOUT AN APPROVED SET OF PLANS AND AN APPROVED PLANTING AGREEMENT. NO IRRIGATION (SPRINKLER) SYSTEMS, BRICK COLUMNS, ENDWALLS, AND/OR BRICK MAILBOXES WILL BE CONSTRUCTED OR INSTALLED WITHIN STATE MAINTAINED RIGHT OF WAY LIMITS WITHOUT A PERMIT. ANY OF THE ABOVE OR OTHER NON-STANDARD ITEMS FOUND IN THE RIGHT OF WAY WITHOUT A PERMIT WILL BE REMOVED, AND ALL COSTS OF THE REMOVAL WILL BE BORNE BY THE OWNER AND/OR DEVELOPER.
- 4.3 THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND PROTECTING ALL UNDERGROUND AND OVERHEAD UTILITIES. WHETHER OR NOT THEY ARE SHOWN ON THE PLANS, PRIOR TO CONSTRUCTION THE CONTRACTOR WILL BE RESPONSIBLE FOR REPAIRS AT HIS OWN EXPENSE OF ANY UTILITIES DAMAGED BY HIS CONSTRUCTION METHODS. MISS UTILITY MUST BE CONTACTED AT 811 AT LEAST 72 HOURS PRIOR TO THE COMMENCEMENT OF CONSTRUCTION.
- 4.4 AUTHORIZED UNDERGROUND UTILITY INSTALLATIONS SHALL MAINTAIN A MINIMUM OF 36 INCHES OF COVER.
- 4.5 OVERHEAD UTILITY INSTALLATIONS WITHIN LIMITED ACCESS AND NON-LIMITED ACCESS RIGHT OF WAY SHALL BE INSTALLED IN ACCORDANCE WITH VDOT LAND USE PERMIT REGULATIONS.
- 4.6 ALL ROADWAY LIGHTING SHALL BE DESIGNED IN ACCORDANCE WITH IESNA LIGHTING CRITERIA AND INSTALLED IN ACCORDANCE WITH VDOT ROAD AND BRIDGE SPECIFICATIONS. ALL ROADWAY LIGHTING SHALL ADHERE TO THE VIRGINIA LIGHTING LAW.
- 5.0 TRAFFIC
- 5.1 THE CONTRACTOR IS RESPONSIBLE FOR THE REPLACEMENT OF ANY PAVEMENT MARKINGS DISTURBED BY CONSTRUCTION. THE CONTRACTOR IS RESPONSIBLE FOR INSTALLING PAVEMENT MARKINGS ON FINAL PAVEMENT OVERLAY WITHIN 14 DAYS AFTER COMPLETION OF SURFACE TREATMENT. PAVEMENT MARKINGS SHALL BE INSTALLED AND ERADICATED IN ACCORDANCE WITH VDOT STANDARDS AND SPECIFICATIONS.
- 5.2 CONTACT TRAFFIC ENGINEERING OPERATIONS AT (757) 925-1659 OR (757) 925-1653 A MINIMUM OF 48 HOURS IN ADVANCE WHENEVER EXCAVATION IS WITHIN 500 FEET OF A TRAFFIC SIGNAL, SO THE LINES CAN BE MARKED. FAILURE TO DO SO COULD BE A COSTLY REPAIR FOR THE DEVELOPER.
- 5.3 CONTACT ROADWAY LIGHTING OPERATIONS AT (757) 484-9015 OR (757) 484-8940 A MINIMUM OF 48 HOURS IN ADVANCE WHENEVER PLANNED CONSTRUCTION ACTIVITIES ARE LOCATED WITHIN LIMITED ACCESS HIGHWAYS. FAILURE TO DO SO COULD BE A COSTLY REPAIR FOR THE DEVELOPER.
- 5.4 CONTACT TRAFFIC OPERATIONS CENTER MAINTENANCE MANAGER AT (757) 424-9903 A MINIMUM OF 48 HOURS IN ADVANCE OF ALL CONSTRUCTION ACTIVITIES LOCATED WITHIN LIMITED ACCESS HIGHWAYS. FAILURE TO DO SO COULD BE A COSTLY REPAIR FOR THE DEVELOPER.
- 5.5 SIGHT DISTANCES AT ENTRANCES AND INTERSECTIONS SHALL BE MAINTAINED AT ALL TIMES DURING AND AFTER CONSTRUCTION. ANY OBJECT OR LANDSCAPING THAT OBSTRUCTS DRIVER VIEW SHALL BE RELOCATED AT THE DEVELOPER'S EXPENSE OR THE ENTRANCE MAY BE CLOSED AT VDOT'S DISCRETION.
- 5.6 WHEN WORKING WITHIN VDOT RIGHT OF WAY, ALL TRAFFIC CONTROL, WHETHER PERMANENT OR TEMPORARY, SHALL BE IN ACCORDANCE WITH THE CURRENT EDITION OF VDOT'S WORK AREA PROTECTION MANUAL.
- 5.7 CONTRACTOR IS RESPONSIBLE FOR THE MAINTENANCE OF ALL EXISTING SIGNS AND POSTS IN THE WORK ZONE. THIS INCLUDES, BUT IS NOT LIMITED TO, STOP, YIELD, STREET NAME, AND SPEED LIMIT SIGNS. ANY SIGN OR POST DAMAGED DURING CONSTRUCTION OR MISSING SHALL BE REPLACED BY THE CONTRACTOR AT HIS EXPENSE.
- 6.0 ENVIRONMENTAL
- 6.1 THE PERMITTEE IS RESPONSIBLE FOR PURSUING AND OBTAINING ANY AND ALL ENVIRONMENTAL CLEARANCES AND/OR PERMITS, INCLUDING, BUT NOT LIMITED TO, WATER QUALITY, THREATENED AND ENDANGERED SPECIES, HAZARDOUS MATERIALS, AND CULTURAL RESOURCES, REQUIRED TO PURSUE THE PROPOSED ACTIVITY BEFORE ANY CONSTRUCTION IS STARTED WITHIN STATE MAINTAINED RIGHT OF WAY LIMITS. DOCUMENTS RELATED TO THESE ACTIVITIES SHALL BE SUBMITTED WITH THE LAND USE PERMIT APPLICATION.
- 7.0 PROJECT CLOSEOUT
- 7.1 INSPECTION DOCUMENTATION FOR STREETS TO BE ACCEPTED FOR STATE MAINTENANCE WILL BE REQUIRED IN ACCORDANCE WITH THE LAND DEVELOPMENT INSPECTION DOCUMENTATION BEST PRACTICES MANUAL. A POST INSTALLATION VISUAL/VIDEO INSPECTION SHALL BE CONDUCTED BY THE CONTRACTOR ON ALL PIPES IDENTIFIED ON THE PLANS AS STORM SEWER PIPE AND A SELECT NUMBER OF PIPE CULVERTS. ADDITIONAL TESTING MAY BE REQUIRED AS DIRECTED BY THE AREA LAND USE ENGINEER OR OTHER REPRESENTATIVE.
- 7.2 THE PERMITTEE SHALL SUBMIT AN AS-BUILT DRAWING OF THE COMPLETED IMPROVEMENTS WITHIN AND ADJACENT TO THE PUBLIC RIGHT OF WAY UNLESS WAIVED BY VDOT. THE AS-BUILT DRAWING INFORMATION SHALL BE PREPARED USING THE SAME VERTICAL DATUM AS THAT USED TO DEVELOP THE ORIGINAL APPROVED PLAN. THE AS-BUILT DRAWING SHALL BEAR THE SEAL OF A PROFESSIONAL ENGINEER OR LAND SURVEYOR REGISTERED IN VIRGINIA.
- 7.3 PRIOR TO RETURN OF THE PROJECT SURETY, THE PERMITTEE SHALL, IN A MANNER THAT IS SATISFACTORY TO VDOT, COMPLETE ALL PUNCHLIST ITEMS AS IDENTIFIED BY VDOT IN WRITING. COMPLETION OF ALL PUNCHLIST WORK WITHIN THE RIGHT OF WAY SHALL BE PERFORMED UNDER AN ACTIVE LAND USE PERMIT.



Rev.	Date	Description
1	07/21/2022	REVISED PER 1ST SUBMITTAL ACC COMMENTS



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SITE PLAN FOR
COLONIAL VETERINARY CLINIC PARKING EXPANSION

Project Contacts: TRS
Project Number: W10705-00
Scale: Date: 01/12/2022
Sheet Title: TRAFFIC CONTROL NOTES AND DETAILS
Sheet Number: C6.2

JAMES CITY COUNTY LANDSCAPE REQUIREMENTS

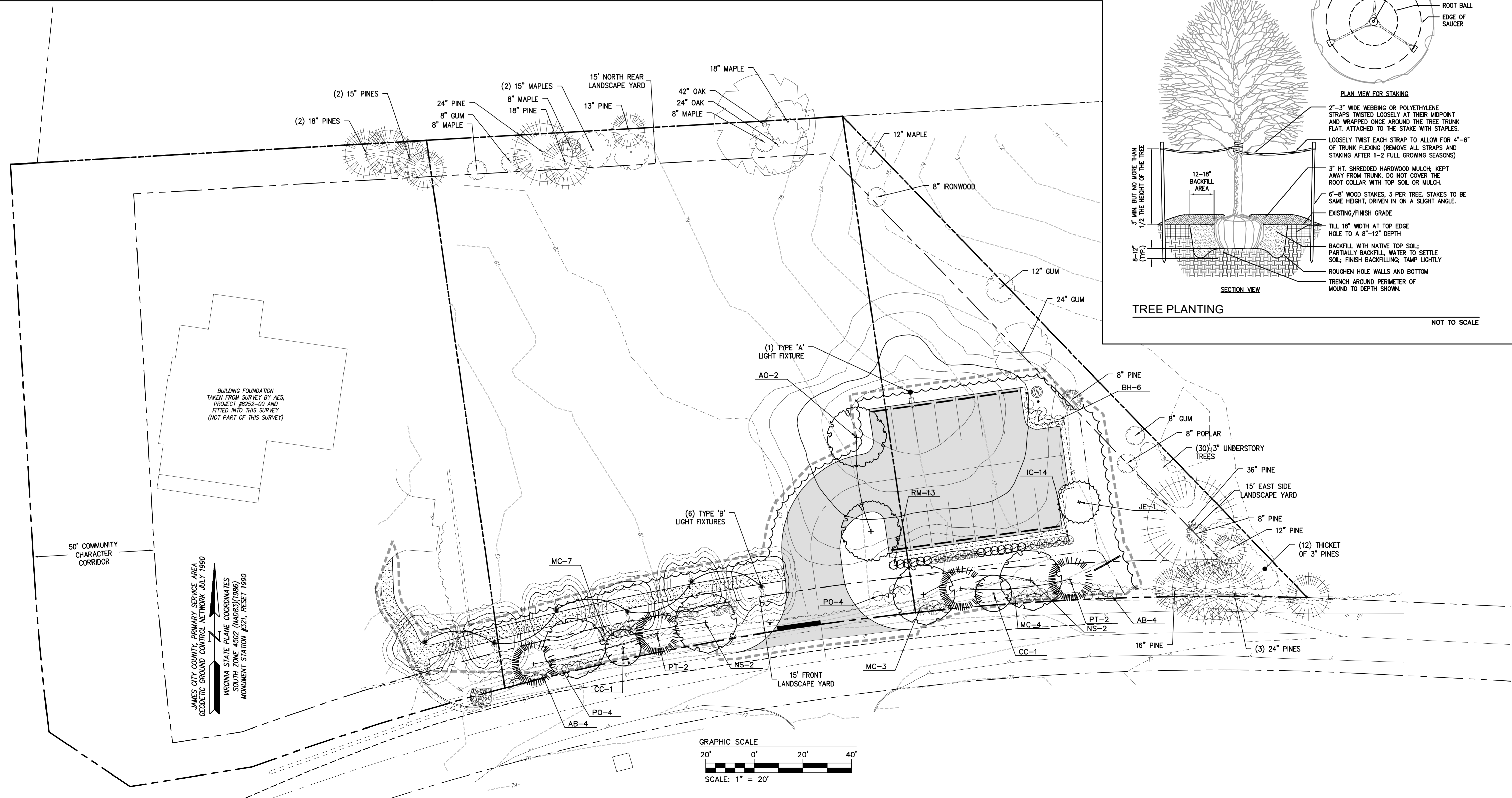
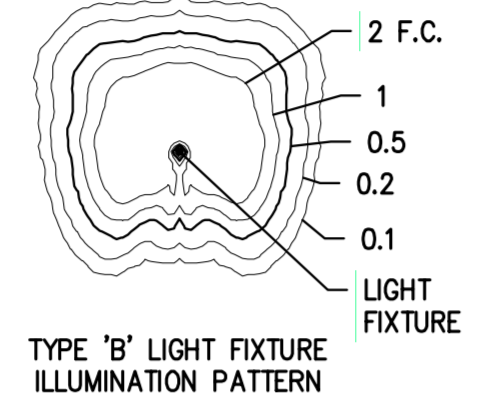
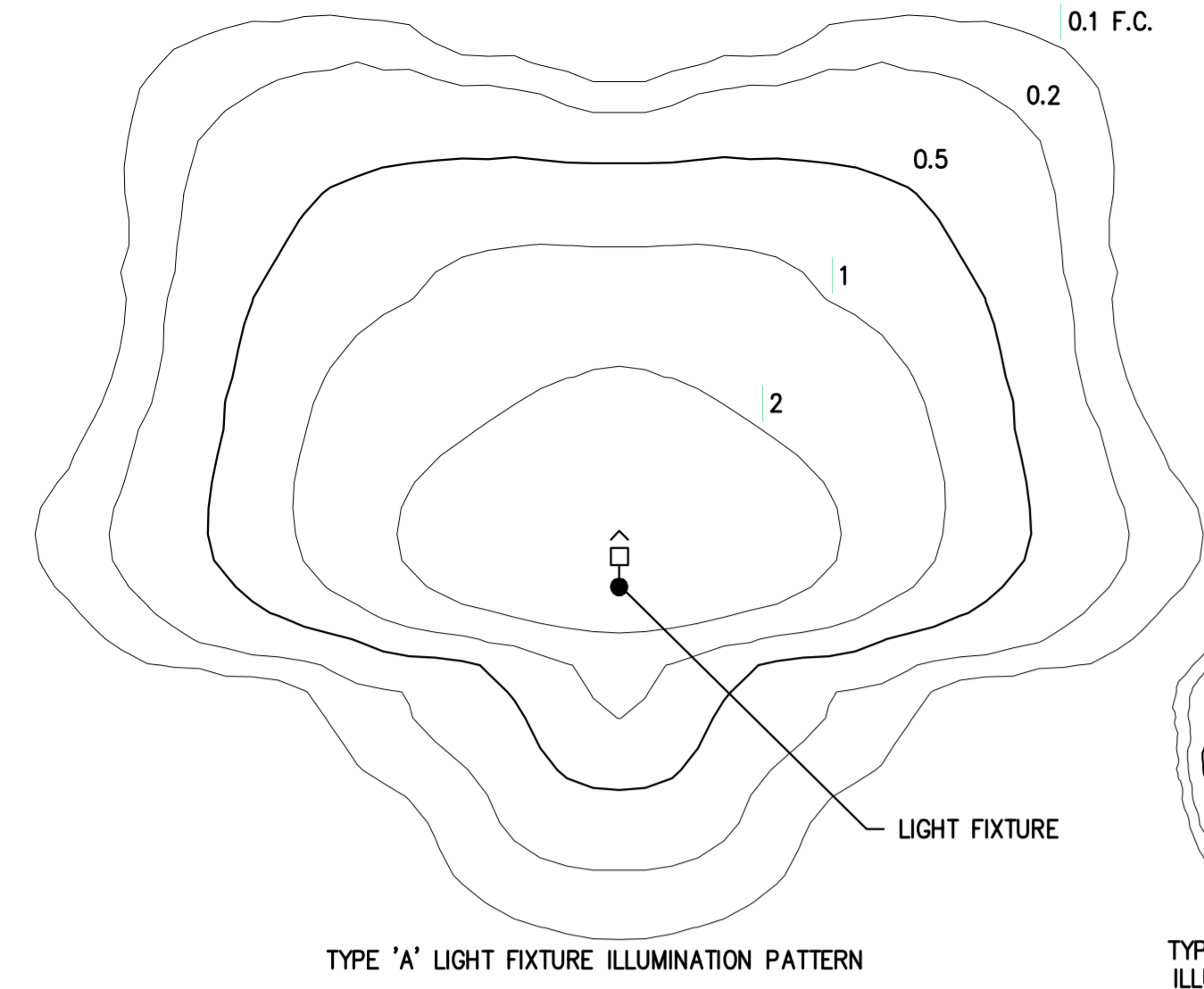
AREAS		TREES AND SHRUBS			
LANDSCAPE YARD	COUNTY REQ.	PLANTING RATIO	PLANTS REQUIRED	EXISTING PLANTS TO REMAIN	NEW PLANTS PROVIDED
15' FRONT YARD INGRAM ROAD	4,910 SF* 3,983 SF* (CLEARED)	1 TREE AND 3 SHRUBS: 400 SQ.FT.	10 TREES 30 SHRUBS	4 TREES 10 TREE CREDITS	10 TREES 30 SHRUBS
15' SIDE YARD EAST SIDE	3,652 SF	1 TREE AND 3 SHRUBS: 400 SQ.FT.	9 TREES 28 SHRUBS	10 TREES 22 TREE CREDITS	--- TREES --- SHRUBS
15' REAR YARD NORTH SIDE	2,515 SF	1 TREE AND 3 SHRUBS: 400 SQ.FT.	7 TREES 19 SHRUBS	19 TREES 29 TREE CREDITS	--- TREES --- SHRUBS
PARKING LOT	15 SPACES	1 TREE AND 2 SHRUBS: 5 PARKING SPACES	3 TREES 6 SHRUBS	--- TREES --- SHRUBS	3 TREES 6 SHRUBS

GENERAL NOTE: THE REMAINING WOODLINE FOR EACH LANDSCAPE YARD CONSISTS OF ENOUGH PLANT MATERIAL TO MEET OR EXCEED THE PLANTING REQUIREMENTS FOR EACH LANDSCAPE YARD.
 * DUE TO THE MAJORITY OF THE FRONT YARD BEING CLEARED, THE PROPOSED LANDSCAPING FOR THE FRONT YARD REFLECTS THE AREA CLEARED. THE EXISTING PORTION TO REMAIN, HAS ENOUGH TREE CREDITS TO SATISFY THE REQUIREMENT FOR THE ENTIRE BUFFER.

GENERAL NOTE: OF THE 26 EXISTING TREES NOTED ON THE PLAN TO REMAIN FOR CREDIT, 14 TREES ARE DECIDUOUS (53.8%), 1 IS ORNAMENTAL (3.9%) AND 11 ARE EVERGREEN (42.3%). SMALLER TREES, NOT TAKEN FOR CREDIT IN THE REMAINING WOOD LINE, CONSIST OF HARDWOODS AND EVERGREEN SPECIES.

PLANT SCHEDULE

EVERGREEN TREES	QTY	BOTANICAL NAME	COMMON NAME	SIZE	ROOT	REMARKS
JE	1	JUNIPERUS VIRGINIANA	EASTERN RED CEDAR	8' HT.	B&B/CONT.	SINGLE STEM
PT	4	PINUS THUNBERGII	JAPANESE BLACK PINE	8' HT.	B&B/CONT.	SINGLE STEM
ORNAMENTAL TREES	QTY	BOTANICAL NAME	COMMON NAME	SIZE	ROOT	REMARKS
CC	2	CERCIS CANADENSIS 'FOREST PANSY'	FOREST PANSY EASTERN REDBUD	8' HT.	B&B/CONT.	MULTI-STEM
DECIDUOUS TREES	QTY	BOTANICAL NAME	COMMON NAME	SIZE	ROOT	REMARKS
AO	2	ACER RUBRUM 'OCTOBER GLORY'	OCTOBER GLORY RED MAPLE	2-1/2" CAL.	B&B/CONT.	SINGLE STEM
NS	4	NYSSA SYLVAICA	BLACK GUM	2-1/2" CAL.	B&B/CONT.	SINGLE STEM
DECIDUOUS SHRUBS	QTY	BOTANICAL NAME	COMMON NAME	SIZE	ROOT	REMARKS
AB	8	ARONIA ARBUTIFOLIA 'BRILLIANTISSIMA'	BRILLIANT RED CHOKEBERRY	22" HT./W.	CONT.	DENSE, FULL
PO	8	PRUNUS LAUROCERASUS 'OTTO LUYKEN'	LUYKENS LAUREL	18" HT./W.	CONT.	DENSE, FULL
EVERGREEN SHRUBS	QTY	BOTANICAL NAME	COMMON NAME	SIZE	ROOT	REMARKS
IC	14	ILEX CORNUTA 'CARISSA'	CARISSA HOLLY	18" HT./W.	CONT.	DENSE, FULL
BH	6	ILEX VOMITORIA 'BORDEAUX'	BORDEAUX HOLLY	18" HT./W.	CONT.	DENSE, FULL
MC	14	MYRTICA CERIFERA	WAX MYRTLE	18" HT./W.	CONT.	DENSE, FULL
RM	13	RHAPHIOLEPIS INDICA 'MONTO' TM	INDIAN PRINCESS INDIAN HAWTHORN	18"-24" HT./W.	CONT.	DENSE, FULL



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SITE PLAN FOR
COLONIAL VETERINARY CLINIC PARKING EXPANSION

JAMES TOWN DISTRICT | JAMES CITY COUNTY | VIRGINIA

Project Contacts: TRS
 Project Number: W10705-00
 Scale: 1"=20' Date: 01/12/2022

Sheet Title:
LANDSCAPE AND LIGHTING PLAN

Sheet Number
L1.0



RSX1 LED Area Luminaire

Specifications

Beam Angle: 0.57° (0.05 mrad)

Length: 21.8" (55.4 cm) (SMA mount)

Width: 13.3" (33.8 cm)

Height: 3.0" (7.6 cm) Main Body, 2.2" (5.6 cm) Arm

Weight (SMA mount): 22.0 lbs (10.0 kg)

Introduction

The new RSX LED Area family delivers maximum value by providing significant energy savings, long life and outstanding photometric performance at an affordable price. The RSX1 delivers 7,000 to 17,000 lumens allowing it to replace 70W to 400W HID luminaires.

The RSX features an integral universal mounting mechanism that allows the luminaire to be mounted on most existing drill hole patterns. This "no-drill" solution provides significant labor savings. An easy-access door on the bottom of mounting arm allows for wiring without opening the electrical compartment. A mast arm adaptor, adjustable integral splitter and other mounting configurations are available.

Ordering Information EXAMPLE: RSX1 LED P4 40K R3 MVOLT SVA DDBXD

Series	Performance Package	Color Temperature	Beam Angle	Mounting	Mounting
RSX1 LED	P1	3000K	R02	1000	MA
	P2	4000K	R02	1000	MA
	P3	5000K	R02	1000	MA
	P4	5000K	R05	1000	MA

Options

Shipped Installed

- HS: Holes (see table)
- PE: Photometric external ballast
- PEE: Photometric external ballast, adjustable
- PF: Switch with back lock (optional only on RSX1)
- CEH: Central entry (1" x 1/2")
- DF: Single foot (12, 27, 34)
- SP: Single foot (12, 27, 34)
- SPOW: 3000 Super pole (120" standard)
- DMG: 3-1/8" diameter pole and back (optional on RSX1)

Shipped Separately

- SS: 1/2" diameter pole and back (optional on RSX1)
- SSW: 1/2" diameter pole and back (optional on RSX1)
- SSS: 1/2" diameter pole and back (optional on RSX1)

LITHONIA LIGHTING Commercial Outdoor

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D-Series LED Bollard

Specifications

Height: 42"

Weight: 27 lbs

Introduction

The D-Series LED Bollard is a stylish, energy-saving, long-life solution designed to perform the way a bollard should—with zero uplift. An optional base for masonry (that fall out of luminaire) will meet the most stringent of lighting codes. The D-Series LED Bollard's rugged construction, durable finish and long-lasting LEDs will provide years of maintenance-free service.

Ordering Information EXAMPLE: D5XB LED 16C 700 40K SYM MVOLT DDBXD

Series	LEDs	Power (watt)	Color Temperature	Beam Angle	Mounting	Mounting
D5XB LED	Asymmetric	350	3500K	30K	3000K	ASY Symmetric
	Symmetric	350	3500K	30K	3000K	SYM Symmetric
	Asymmetric	350	4000K	30K	3000K	ASY Symmetric
	Symmetric	350	4000K	30K	3000K	SYM Symmetric

Accessories

Shipped Separately

- SS: 1/2" diameter pole and back (optional on RSX1)
- SSW: 1/2" diameter pole and back (optional on RSX1)
- SSS: 1/2" diameter pole and back (optional on RSX1)

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SSS Square Straight Steel Poles TYPE A POLE

Example: SSS 20 SC CM10 DDB

Series	Height	Material	Finish	Options	Notes
SSS	10'	4" x 4" (101.6mm)	Galvalume	Standard	1. Standard
	12'	4" x 4" (101.6mm)	Galvalume	Standard	2. Standard
	14'	4" x 4" (101.6mm)	Galvalume	Standard	3. Standard
	16'	4" x 4" (101.6mm)	Galvalume	Standard	4. Standard
	18'	4" x 4" (101.6mm)	Galvalume	Standard	5. Standard
	20'	4" x 4" (101.6mm)	Galvalume	Standard	6. Standard
	22'	4" x 4" (101.6mm)	Galvalume	Standard	7. Standard
	24'	4" x 4" (101.6mm)	Galvalume	Standard	8. Standard
	26'	4" x 4" (101.6mm)	Galvalume	Standard	9. Standard
	28'	4" x 4" (101.6mm)	Galvalume	Standard	10. Standard
	30'	4" x 4" (101.6mm)	Galvalume	Standard	11. Standard
	32'	4" x 4" (101.6mm)	Galvalume	Standard	12. Standard

Notes

- Standard finish is Galvalume. Other finishes are available.
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TYPE A

Performance Data

Lumen Output

Table with columns: Pole Height, Pole Diameter, Luminaire Type, Luminaire Power, Luminaire Beam Angle, Luminaire Mounting, Luminaire Output, Pole Output.

TYPE B

Performance Data

Lumen Output

Table with columns: Pole Height, Pole Diameter, Luminaire Type, Luminaire Power, Luminaire Beam Angle, Luminaire Mounting, Luminaire Output, Pole Output.

SSS Square Straight Steel Poles TYPE A POLE

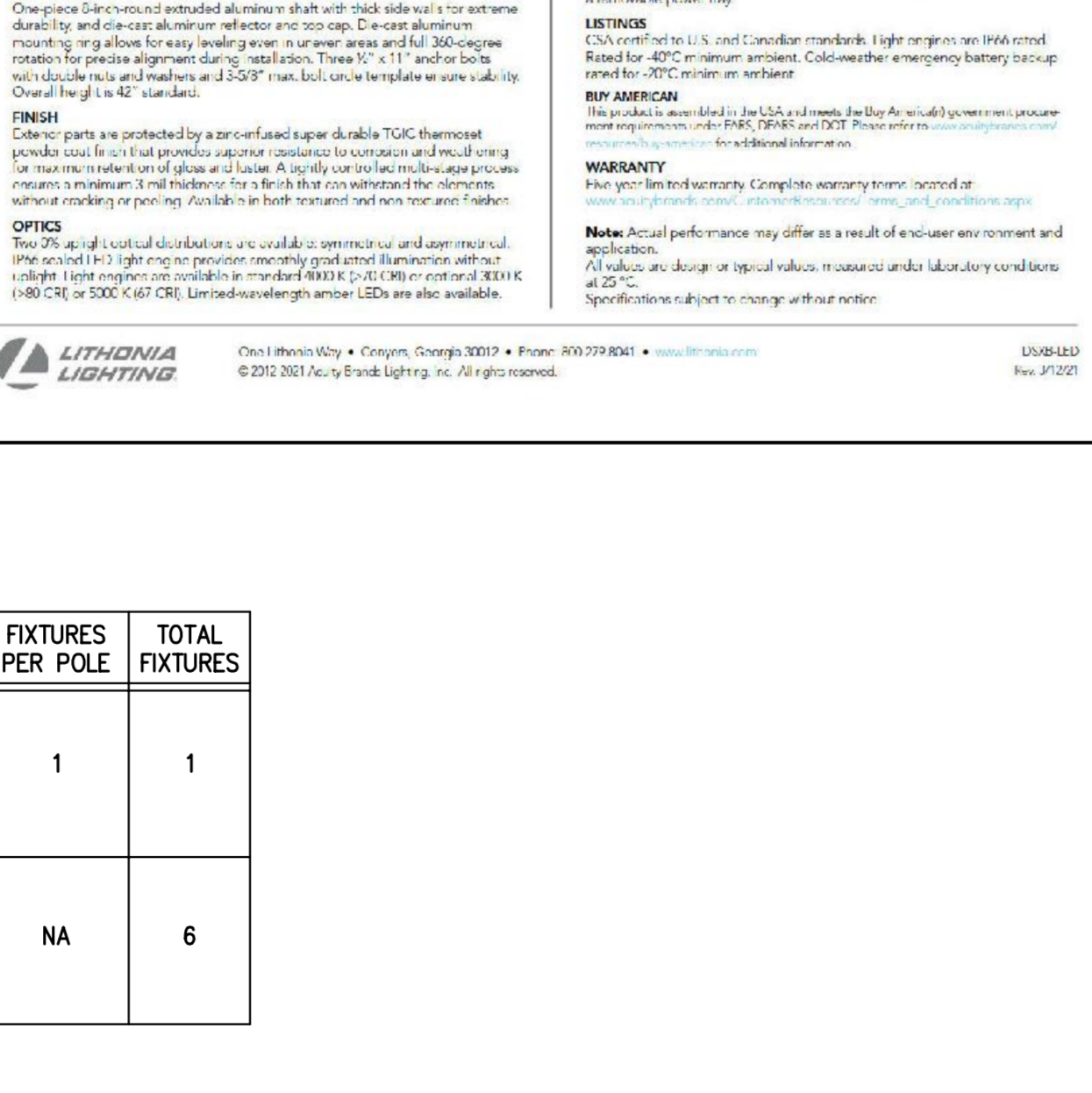
TECHNICAL INFORMATION - EPA (F1) with 1.2 gust

Catalog Number	Height	Material	Finish	Options	Notes
SSS10C	10'	4" x 4" (101.6mm)	Galvalume	Standard	1. Standard
SSS12C	12'	4" x 4" (101.6mm)	Galvalume	Standard	2. Standard
SSS14C	14'	4" x 4" (101.6mm)	Galvalume	Standard	3. Standard
SSS16C	16'	4" x 4" (101.6mm)	Galvalume	Standard	4. Standard
SSS18C	18'	4" x 4" (101.6mm)	Galvalume	Standard	5. Standard
SSS20C	20'	4" x 4" (101.6mm)	Galvalume	Standard	6. Standard
SSS22C	22'	4" x 4" (101.6mm)	Galvalume	Standard	7. Standard
SSS24C	24'	4" x 4" (101.6mm)	Galvalume	Standard	8. Standard
SSS26C	26'	4" x 4" (101.6mm)	Galvalume	Standard	9. Standard
SSS28C	28'	4" x 4" (101.6mm)	Galvalume	Standard	10. Standard
SSS30C	30'	4" x 4" (101.6mm)	Galvalume	Standard	11. Standard
SSS32C	32'	4" x 4" (101.6mm)	Galvalume	Standard	12. Standard

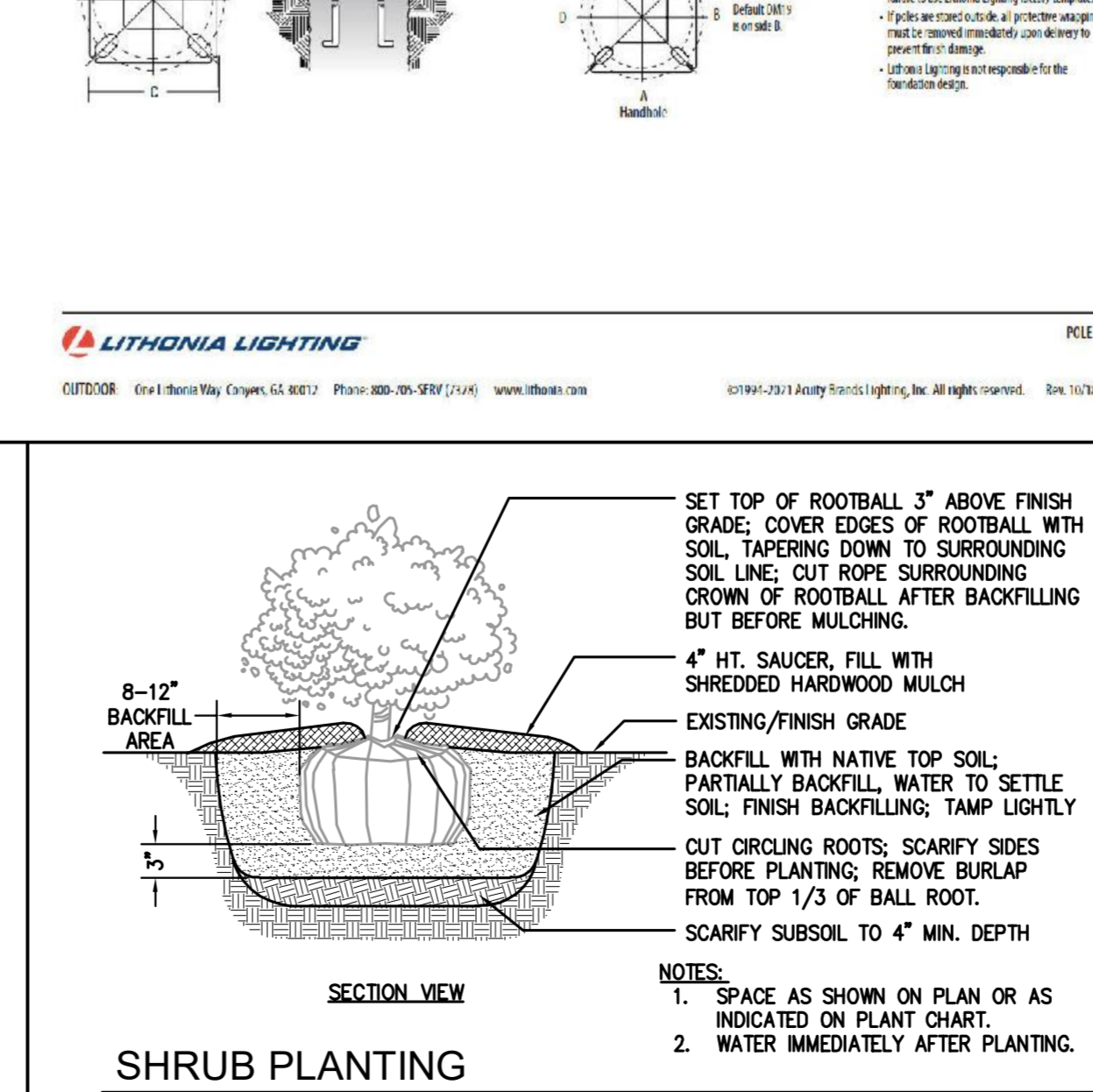
LIGHTING SCHEDULE

KEY	FIXTURE TYPE	MOUNTING HEIGHT	POLE TYPE/ MOUNTING INSTRUCTIONS	TOTAL POLES	FIXTURES PER POLE	TOTAL FIXTURES
A	LITHONIA LIGHTING RSX1 LED-P2-40K-R4-DDBXD RSX1 AREA LIGHT, 72 WATT, 4000K COLOR TEMP, TYPE 4 DISTRIBUTION, B.U.G. RATING: 2-0-2, COLOR: DARK BRONZE	18'	LITHONIA LIGHTING SSS-18-4C-DDBXD SQUARE STRAIGHT STEEL POLE, FLUSH POLE BASE, COLOR: DARK BRONZE	1	1	1
B	LITHONIA LIGHTING DSXB LED-12C-350-40K-ASY-DDBXD D-SERIES BOLLARD, 12 LEDS, 16 WATT, 4000K COLOR TEMP, B.U.G. RATING: 1-0-1, COLOR: DARK BRONZE	42"	BOLLARD FIXTURE	NA	NA	6

SHRUB PLANTING



FLUSH POLE BASE DETAIL



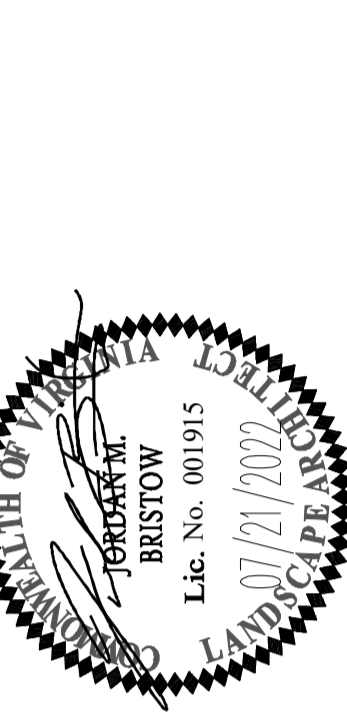
GENERAL NOTES

- ALL PLANT STOCK SHALL MEET THE MINIMUM STANDARDS & SPECIFICATIONS DESCRIBED IN THE "AMERICAN STANDARD FOR NURSERY STOCK," LATEST EDITION, PUBLISHED BY THE AMERICAN ASSOCIATION OF NURSERYMEN.
- ALL PLANT MATERIAL SHALL BE INSTALLED AS SPECIFIED IN THE VNLA STANDARDIZED LANDSCAPE SPECIFICATIONS, LATEST EDITION.
- THE CONTRACTOR SHALL SUPPLY ALL NEW PLANT MATERIAL IN QUANTITIES SUFFICIENT TO COMPLETE ALL PLANTING SHOWN ON THE DRAWINGS. WHERE DISCREPANCIES EXIST BETWEEN THE PLANS & THE PLANT LIST, THE PLANS SHALL TAKE PRECEDENCE.
- GROUPINGS OF PLANTS SHALL BE MULCHED IN CONTINUOUS PLANT BEDS.
- AREAS DISTURBED BY CONSTRUCTION, NOT OTHERWISE WITHIN PLANT BEDS OR COVERED IN SITE CONTRACT, ARE TO BE SODED OR SEEDING WITH A STATE CERTIFIED TURF-TYPE TALL FESCUE VARIETY SELECTED FROM THE FOLLOWING LIST:
 - Bltmore, Bingo, Cochie II, Constitution, Coyote II, Crossfire II, Endeavor, Fidelity, Good-en, Grande, Greenkeeper WAF, Inferno, Kalahari, Magellan, Masterpiece, Onyx, Padre, Picasso, Penn 1901, Quest, Raptor, Rebel Exedo, Rembrandt, Rendition, SR 8250, SR 8300, Torhee, Titanium, Watchdog, Wolfpack, WPEZE.
- AREAS OF THE SITE DEPICTED AS "TURF" REPRESENT MAINTAINED TURF AND MAY INCLUDE ALREADY ESTABLISHED AREAS. THE PROVISION OF TURF AREAS BY SEEDING OR SODDING SHALL BE DETERMINED AT THE OWNER'S DISCRETION.
- TREES SUPPORT STAKING IS OPTIONAL FOR TREES THAT ARE 1" CAL. OR 6' HT. OR LESS. ALL TREE STAKING SHALL BE REMOVED AFTER 1-2 GROWING SEASONS.
- ALL TREES ARE TO BE PLANTED SO TOP OF ROOT BALL IS 3" ABOVE FINISHED GRADE.
- TREE SHALL BE INSTALLED PLUMB & STRAIGHT.
- PRUNE ALL SUCKERS, RUBBING OR CROSSED BRANCHES, CODOMINANT LEADERS, NARROW CROTCH ANGLES, WATER SPROUTS, BROWN BRANCHES.
- DO NOT PRUNE CENTRAL LEADER OR BRANCH TIPS.
- REMOVE TAGS, LABELS & PLASTIC SLEEVING.
- DO NOT WRAP TRUNK.
- IF ANY PLANT MATERIAL IS CONTAINER-GROWN, REMOVE TOP OF WIRE BASKET, OR REMOVE CONTAINER & CUT CIRCLING ROOT; IF FIELD-GROWN, CUT ROPE SURROUNDING BOTTOM OF TREE TRUNK AFTER BACKFILLING BUT BEFORE MULCHING & REMOVE BURLAP FROM TOP 1/3 OF BALL ROOT.
- REMOVE ALL STAKES, STRAPS, WIRES, RUBBER HOSES, ETC. AFTER 1-2 GROWING SEASONS.
- PLANT SUBSTITUTIONS SHALL NOT BE MADE WITHOUT THE WRITTEN CONSENT OF THE OWNER OR THE OWNER'S DESIGNATED REPRESENTATIVE PRIOR TO INSTALLATION.
- ALL INSTALLED PLANT MATERIAL SHALL BE SUBJECT TO REGULAR MAINTENANCE, INCLUDING FERTILIZATION, PRUNING, REPLACEMENT, INSECT AND DISEASE CONTROL, WATERING, MULCHING, AND WEED CONTROL.
- CONTRACTORS ARE RESPONSIBLE FOR LOCATING ALL UTILITIES PRIOR TO THE BEGINNING OF WORK AND AVOIDING THEM DURING LANDSCAPING OPERATIONS.
- ALL B&B AND CONTAINER PLANTS SHALL BE PLANTED BETWEEN MARCH 15-JUNE 30 OR SEPTEMBER 15-NOVEMBER 15. GRASSES SHALL BE PLANTED IN THE SPRING.
- ACCEPTABLE SURVIVAL RATES AT THE END OF A GROWING SEASON FOR ALL WOODY PLANT MATERIAL IS 100% AND 85% FOR ALL HERBACEOUS PLANT MATERIAL.

LIGHTING NOTES

- THE LIGHTING PLAN IS INTENDED SOLELY FOR THE PURPOSE OF FIXTURE SELECTION & PLACEMENT AND DEPICTING ASSOCIATED LUMINANCE LEVELS.
- THE CONTRACTOR AND/OR ELECTRICAL ENGINEER SHALL BE RESPONSIBLE FOR: SOURCE OF ELECTRICAL POWER; CIRCUITRY; WIRE SIZE; CONDUIT LAYOUT; AND ANY OTHER ELECTRICAL REQUIREMENTS.
- LIGHT POLE BASES FLUSH WITH FINISHED GRADE LOCATED ON THE PERIMETER OF A PARKING LOT AREA OR WITHIN A PARKING LOT ISLAND SHALL BE INSTALLED TO BE CENTERED A MINIMUM OF 4" FROM THE BACK-OF-CURB AND SHALL BE LOCATED "IN-LINE" WITH PARKING LOT STRIPING WHERE APPLICABLE. WHERE THIS MINIMUM DISTANCE CAN NOT BE MET, A 30" CONCRETE BASE SHALL BE INSTALLED. THE CONTRACTOR SHALL ENSURE THAT THE OVERALL MOUNTING HEIGHT IS STILL ACHIEVED AND/OR NOT EXCEEDED WITH THE ADDITIONAL HEIGHT OF THE 30" CONCRETE BASE.
- LIGHT POLE BASES LOCATED WITHIN A PARKING LOT PAVED AREA SHALL BE CENTERED ON THE INTERSECTION OF THE ASSOCIATED PARKING LOT STRIPING. THE CONCRETE BASES FOR THESE POLES SHALL EXTEND A MINIMUM OF 30" ABOVE FINISHED GRADE. THE CONTRACTOR SHALL ENSURE THAT THE OVERALL MOUNTING HEIGHT IS STILL ACHIEVED AND/OR NOT EXCEEDED WITH THE ADDITIONAL HEIGHT OF THE 30" CONCRETE BASE.
- CONTRACTOR SHALL ENSURE THE LIGHT FIXTURE, POLE, POLE BASE, AND CONCRETE BASE ARE COMPATIBLE.
- INSTALLATION OF POLES AND LIGHTING FIXTURES SHALL BE IN ACCORDANCE WITH MANUFACTURER'S INSTRUCTIONS.
- THE CONTRACTOR SHALL ENSURE THAT THE POLE DIAMETER, GAUGE, AND CONCRETE BASE MEET OR EXCEED THE MINIMUM LOAD REQUIREMENTS BASED ON FIXTURE COUNT, REGIONAL WIND LOAD STATISTICS, ADDITIONAL BANNERS / POLE ARMS, AND THE FIXTURE SPECIFICATIONS.
- CONTRACTORS ARE RESPONSIBLE FOR LOCATING ALL UTILITIES PRIOR TO THE BEGINNING OF WORK AND AVOIDING THEM DURING INSTALLATION OPERATIONS.

REV.	DATE	DESCRIPTION
1	07/21/2022	REVISED PER 1ST SUBMITTAL ACC COMMENTS



JAMES CITY COUNTY, VIRGINIA

LANDSCAPE ARCHITECT

BRISTOW

Lic. No. 001915

THIS PLAN HAS NOT RECEIVED FINAL APPROVAL AND IS NOT APPROVED FOR CONSTRUCTION.

ABS

CONSULTING ENGINEERS

Hampton Roads | Central Virginia | Middle Peninsula

52488 Old Towne Road, Suite 1
Williamsburg, Virginia 23188
Phone: (757) 255-0600
www.abs.com

SITE PLAN FOR COLONIAL VETERINARY CLINIC PARKING EXPANSION

Project Contacts: TRS
Project Number: W10705-00
Scale: 1"=20'
Date: 01/12/2022

Sheet Title: LANDSCAPE AND LIGHTING NOTES AND DETAILS

Sheet Number: **L2.0**

Z-0005-2007

080 007401

Tax Map No (47-1) (1-23)

112 INGRAM ROAD OFFICE BUILDING
PROFFERS

4c COPY

These proffers are made as of this 25th day of January, 2008 by EVANS DEVELOPMENT CORPORATION, (together with its successors and assigns, the "Owner")

RECITALS

- A. Owner is the Owner of a tract or parcel of land located in James City County, Virginia containing approximately 0.37 acres and being James City County Real Estate Tax Parcel (47-1) (1-23) and further described in " Attachment A" ("the Property"). The Property is now zoned R-8, Rural Residential.
- B. Owner has applied to rezone the Property from R-8, Rural Residential, to B-1, General Business, with proffers.
- C. Buyer has submitted to the County a Conceptual Plan entitled "Conceptual Site Plan" prepared by Walters Land Surveying, LTD, dated 8/15/07. ("the Plan")
- D. Owner desires to offer the County certain conditions on the development of the Property not generally applicable to land zoned B-1. Therefore, and in consideration of the approval by the Board of Supervisors of the rezoning, and pursuant to Section 15.2-2303. et seq. of the Code of Virginia, 1950, as amended, and Section 24-15, of the Zoning Ordinance, owner agrees that it shall meet and comply with all of the following conditions in developing the Property. If the requested rezoning is not granted by the County, these Proffers shall be void.

CONDITIONS

1. Uses and Conceptual Plan. The Property shall be put to one or more of the following possible uses: business, governmental, contractor's and professional offices and accessory uses thereto as defined in the James City County Zoning Ordinance. All other otherwise permissible uses shall be prohibited. The Property shall be developed generally in accordance with the Plan, with such minor changes as the Development Review Committee determines does not change the basic concept or character of the development.
2. Architecture. The office building on the Property shall be developed in an architectural design and color scheme generally consistent with the rendering revised November 9, 2007 made by DeBlasio Design and Architects and as shown in the Plan and consistent with the architectural standards set forth in the Primary Principles for Five Forks Area of James City County, adopted by the Board of Supervisors on September 28, 2004. Said design shall be approved by the Planning Director prior to final site plan approval.
3. Landscaping. An enhanced landscaping plan shall be provided to the Planning Director for approval prior to final site plan approval. This plan shall provide enhanced landscaping along the entire road frontage of the property. ("Enhanced landscaping" shall be defined as 125 percent of the zoning ordinance landscape size requirements)
4. Entrance. An entrance constructed in accordance with applicable VDOT standards shall be installed prior to issuance of any Certificate of Occupancy.
5. Intersection Improvements. A cash contribution of \$443.00 shall be made to the County prior to final site plan approval in order to mitigate traffic impacts resulting, in part, from the physical development and operation of the Property. The County shall use these funds towards the construction of intersection improvements to the Ironbound Road/John Tyler Highway intersection as detailed in section I.1 of the Primary Principles for Five Forks Area of James City County, adopted by the Board of Supervisors on September 28, 2004 or toward pedestrian improvements at said intersection.

3 Prepared by: Scott Evans, 5251-18 John Tyler Hwy, #134, Williamsburg, VA 23185.

✓ Return to: JCC Attorney's Office, 101-C Mounts Bay Road, Williamsburg, VA 23185.

6. Water Conservation. The Owner shall be responsible for developing and enforcing water conservation standards to be submitted to and approved by the James City Service Authority prior to final site plan approval. The standards may include, but shall not be limited to such water conservation measures as limitations on the installation and use of irrigation systems and irrigations wells, the use of approved landscaping materials including the use of drought tolerant plants, warm season grasses and the use of water conserving fixtures and appliances to promote water conservation and minimize the use of public water resources.

7. Irrigation: In the design phase, the developer and designing engineer shall include the design of stormwater systems that can be used to collect stormwater for outdoor water use for the entire development. Only surface water collected from surface water impoundments (the "Impoundments") may be used for irrigating common areas on the Property (the "Irrigation"). In no circumstances shall James City Service Authority (the "JCSA") water or well water be used for Irrigation, except as otherwise provided by this condition. If the Owner demonstrates to the satisfaction and approval of the General Manager of the JCSA that the Impoundments cannot provide sufficient water for Irrigation, the General Manager of the JCSA may, in writing, approve a shallow (less than 100' feet) well to supplement the water provided by the Impoundments. If water from shallow wells is inadequate to irrigate common areas the General Manager of the JCSA may approve areas after reviewing site plans that demonstrate the common areas incorporate water conservation techniques (i.e. minimize turf, use of drought tolerant plant, drip irrigation, etc).

Witness the following signature and seal

Evans Development Corporation

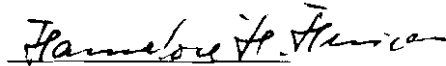


Scott Evans, President

COMMONWEALTH OF VIRGINIA

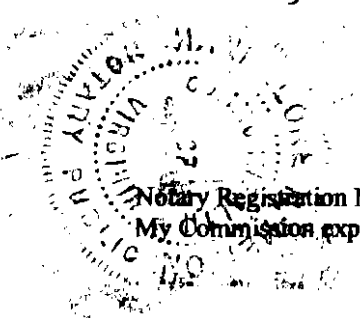
City/County of JC, to wit:

The foregoing instrument was acknowledge before me by SCOTT EVANS, on this 28 day of January, 2008.



Notary Public

Notary Registration Number: 183385
 My Commission expires: 12/31/2009



Prepared by: Scott Evans, 5251-18 John Tyler Hwy, #134, Williamsburg, VA 23185

Return to: James City County Attorneys Office, 101-C Mounts Bay Road, Williamsburg, VA 231285.

EXHIBIT "A"

All that certain lot, piece or parcel of land situate, lying and being in the County of James City, Virginia, and designated as N/P Jenway LP, which contains approximately 0.359 acres more or less, as shown on that certain plat entitled, "A PHYSICAL SURVEY OF PROPERTY LOCATED AT 112 INGRAM ROAD, JAMES CITY COUNTY, VIRGINIA FOR EVANS DEVELOPMENT CORPORATION, JAMES CITY COUNTY, VIRGINIA", prepared by LandTech Resources, Inc., Surveying, Mapping, Land Design, dated March 9, 2006 which plat is duly recorded in the Circuit Court Clerk's Office for the City of Williamsburg and County of James City, Virginia, simultaneously with and immediately prior hereto.

VIRGINIA: CITY OF WILLIAMSBURG & COUNTY OF JAMES CITY
This document was admitted to record on 14 Mar 08
at 9:55 AM/PM. The taxes imposed by Virginia Code
Section 58.1-801, 58.1-802 & 58.1-814 have been paid.

STATE TAX LOCAL TAX ADDITIONAL TAX

\$ _____ \$ _____ \$ _____
TESTE: BETSY B. WOOLRIDGE, CLERK

BY: Betsy B. Woolridge Clerk

RESOLUTION

PRIMARY PRINCIPLES FOR FIVE FORKS AREA OF JAMES CITY COUNTY

- WHEREAS, Economic Development Action 12G of the 2003 Comprehensive Plan recommends that James City County evaluate redevelopment and land use issues in the Five Forks area; and
- WHEREAS, on June 8, 2004, the Board of Supervisors created the Five Forks Area Study Committee to conduct a comprehensive study of the area and develop a set of guiding principles for future development; and
- WHEREAS, these principles will be used by citizens, staff, Planning Commission, and the Board of Supervisors to guide recommendations and decisions in future land use cases and other development activity in the Five Forks area; and
- WHEREAS, after four public meetings the Five Forks Area Study Committee unanimously adopted primary principles for the Five Forks area of James City County; and
- WHEREAS, on September 13, 2004, the James City County Planning Commission recommended the adoption of the primary principles by a vote of 7-0.

NOW, THEREFORE, BE IT RESOLVED that the Board of Supervisors of James City County, Virginia, does hereby endorse the following Vision and Principles to be used alongside the 2003 Comprehensive Plan when reviewing Rezonings, Special Use Permits, and other development activities in the Five Forks area:

Primary Principles for the Five Forks Area of James City County

Five Forks is an area with a unique village character. Bounded to the east by Mill Creek and to the west by the Powhatan Creek, Five Forks is within a significant natural area. Five Forks also supports a thriving commercial center and boasts a quality elementary school at its southern edge. Five Forks is generally understood to encompass the area that lies within three quarters of a mile of the intersection of John Tyler Highway and Ironbound Road.

Five Forks has grown and changed. With new growth, however, come questions about traffic levels, housing capacity, and preservation of the village qualities that make the area unique.

The Five Forks Area Study Committee was created by the Board of Supervisors to listen to the views of County citizens, particularly those who live and work in Five Forks. The Committee's purpose was to recommend principles that preserve and build upon the many positive qualities of Five Forks. These principles seek to protect the watersheds and safeguard the village character of the area. The principles will address residential growth, commercial development, traffic concerns, and alternative transportation. The principles will be incorporated into the next regularly scheduled update of the County's Comprehensive Plan. Until that time, these principles, when approved, serve as an addendum to the 2003 Comprehensive Plan.

Vision Statement

Five Forks has a rich heritage and a community character unique to James City County. By cooperating with citizens and with local government we will preserve these qualities for future generations. Through these principles, the Committee envisions that Five Forks will be a place where future redevelopment or development:

- Improves or maintains water quality and other environmental features;
- Preserves Five Forks' unique village character;
- Does not overburden the road network beyond capacity;
- Provides adequate facilities for pedestrians and cyclists;
- Provides goods and services needed by citizens; and
- Ensures housing opportunities for all citizens.

I. Transportation Principles

1. Capitalize on and Enhance Existing Roadway Network (see the Environmental Principles for relevant information related to these recommended actions):
 - Inventory/validate existing pavement and right-of-way width.
 - Reconfigure pavement markings/lane delineations to accommodate a 150-foot full-width exclusive right-turn lane for southbound Ironbound Road (i.e., north leg).
 - Construct a 150-foot full-width right-turn lane along the northbound approach of Ironbound Road (i.e. south leg).
 - Reduce the speed limit to 35 mph approximately a half mile from the intersection of Ironbound Road and John Tyler Highway.
 - Implement AM, Noon, PM, and Off-Peak signal timing modifications to best process traffic, maximize available and enhanced capacity, and to sustain acceptable level of operations for the isolated signalized intersection of Ironbound Road and John Tyler Highway.
2. In conjunction with any development proposals using Ingram Road West for access, encourage developers to make road improvements (reopening access from Ingram Road East from John Tyler Highway was considered but was not recommended. Such reopening might prove to be unsafe and possible benefits appear to be minimal. The initiative might prove to be beneficial at some time in the future depending on future development on Ingram Road East.):
 - Developers using Ingram Road West for access should rebuild this road as a two-lane roadway in accordance with current VDOT street requirements. Improvements could include:
 - 12 - 14-foot lanes to include roadway as well as curb and gutter;
 - 4-foot buffer between curb and sidewalk on one side of roadway;
 - Street trees and other aesthetic improvements; and
 - 25 mph posted speed limit.

3. Promote pedestrian and bicycle facility interconnectivity within Five Forks area (see the Land Use and Environmental Principles for relevant information related to these recommended actions):

- Utilize available funds in the Sidewalk Capital Improvement Program budget as well as alternate sources of funding including grants or private contributions to construct sidewalks and pedestrian crosswalks in accordance with the phasing plan listed below.
- Ensure that new development either provides sidewalks along public road frontages in accordance with the recommendations of the sidewalk inventory, or contributes funds to the Sidewalk Capital Improvement Program.
- Coordinate the design and construction of roadway improvement projects with bicycle and pedestrian facilities. Bicycle and pedestrian facilities should be designed with an emphasis on safety, adequate lighting, signage, and Americans with Disabilities Act (ADA) compliant features.

Phase I

- Using the Five Forks area sidewalk inventory, and considering existing and potential development, and existing sidewalk connections as a guide, develop an implementation plan to extend sidewalks to serve pedestrian activity within the businesses at the Ironbound Road/John Tyler Highway intersection.
- Stripe crosswalks and provide crossing ramps and pedestrian signals for each approach to the Ironbound Road/John Tyler Highway intersection.
- Provide paved shoulders on John Tyler Highway west of the Ironbound Road intersection during the next VDOT repaving to decrease road maintenance and provide more travel space for bicycles and pedestrians.

Phase II

- Using the Five Forks area sidewalk inventory, existing and potential development, and existing sidewalk connections as a guide, develop an implementation plan to construct sidewalk segments that provide greater connectivity between the central business area and Clara Byrd Baker Elementary School, neighborhoods, and recreational areas.
- In accordance with the Greenway Master Plan, construct a multi-use path along John Tyler Highway that can connect to Jamestown High School and the Greensprings Trail.
- Construct shoulder bikeways along Ironbound Road using Federal grants. In accordance with the Greenway Master Plan, construct a multi-use path along Ironbound Road that can connect to Mid-County Park/Monticello Marketplace Shopping Center.
- Utilize Greenway Funds in the Capital Improvement Program budget and other sources of funding such as grants to support the construction of the above multi-use paths.

4. Promote opportunities for bus service in Five Forks:
 - Work with Williamsburg Area Transport (WAT) to investigate areas and routes with the highest ridership and potential for enhanced service (e.g., to serve activity/employment centers).
 - Work with WAT and Traffix to promote public transportation incentives and the use of alternative commuting modes (park-and-ride, ride sharing, express routes, etc.) to both employers and employees.
 - Investigate opportunities to increase ridership to/from centers of activity, businesses, residential areas and special event attractions.
5. Maintain a "C" level of service for traffic conditions in Five Forks by adhering to new trip generation thresholds established in the Five Forks Area Study Traffic Impacts Alternative Analysis prepared by Kimley Horn and Associates when approving new development through the rezoning and special use permit process (trip levels above the threshold result in the Level of Service decreasing from C to D. These new trip generation threshold numbers are on top of projected 2008 background trips.):
 - Without Geometric Improvements
 - AM peak should not exceed 350 new trips
 - PM peak should not exceed 500 new trips
 - With Geometric Improvements recommended by Principle I.1
 - AM peak should not exceed 500 new trips
 - PM peak should not exceed 650 new trips
 - New development should be phased so that new trips do not exceed the lower thresholds until the improvements listed in Principle I.1 are either constructed or fully funded in the VDOT Six-Year Road Plan.
 - New development should provide a pro-rata share of the costs associated with implementing the geometric and signal improvements.

II. Environmental Principles

1. Maintain and improve water quality and reduce flooding risk in the Mill Creek and Powhatan Creek Watersheds by minimizing the amount of additional impervious cover and treating existing and additional stormwater runoff:
 - Develop a coordinated stormwater master plan for Five Forks. The stormwater master plan should address possibilities for regional treatment or other treatment approaches for new and existing development as well as opportunities to reduce and/or treat runoff from the existing roadway into Powhatan Creek and Mill Creek.
 - Minimize drainage of new sidewalks, multiuse paths, or other transportation improvements. Encourage drainage of these improvements into a treatment facility such as a grassy swale, regional and structural Best Management Practices (BMP), or other appropriate options.

- For new or modified residential or commercial development in the Powhatan Creek and Mill Creek watershed, encourage the use of Low Impact Design (LID) and Better Site Design (BSD) techniques such as, but not limited to, those listed in the 2003 Comprehensive Plan; the Builders for the Bay James City County Local Site Planning Roundtable consensus document (expected to be completed in Fall 2004); and the booklet entitled "*Better Site Design: An Assessment of the Better Site Design Principles for Communities Implementing Virginia's Chesapeake Bay Preservation Act.*"
 - Work with the Village Square Homeowners Association to ensure maintenance of the Village Square BMP and encourage the community to improve the existing BMP by pursuing a grant through the County PRIDE mini-grant program. Explore options for retrofitting and/or maintaining other Five Forks area BMPs.
 - Investigate options for and encourage the undertaking of stream restoration projects in the Powhatan Creek and Mill Creek Watersheds.
2. Ensure that any new development in the Powhatan Creek Watershed implements the recommendations of the Powhatan Creek Watershed Management Plan adopted by the Board of Supervisors on February 26, 2002:

Watershed Management Plan Recommendations:

- Non-tidal mainstem in the Five Forks area (west of Ironbound and north of Ingram Road):
By encouraging the use of expanded buffers along the Powhatan Creek mainstem.
 - Tidal mainstem in the Five Forks area (west of Ironbound Road and south of Ingram Road):
By encouraging the use of expanded buffers along the Powhatan Creek mainstem stormwater management with an added focus on fecal coliform removal.
 - Stormwater Recommendations: Use of Special Stormwater Criteria; specialized on-site BMP design with emphasis on removal of nutrients and bacteria; minimize stormwater outfalls on steep slopes.
3. Explore options for land conservation in Five Forks:
- Through the rezoning and special use permit process; encourage developers to set aside land as permanent open space.
 - Continue to target County Green Space Acquisition Funds to acquire properties that are environmentally sensitive or preserve the John Tyler Highway Community Character Corridor.

III. Land Use Principles

1. Promote mixed-use, pedestrian-friendly land-use patterns (see Principles III.6 for Land Use recommendations, including recommendations on moderate- and low-income housing):
- Pursue regulatory and investment strategies that promote a safe and healthy mix of uses (e.g., retail, residential, office, and public facilities).

- Continue to promote Five Forks as a center of community activity with complementary mixed uses.
 - Promote development patterns that support compact development, interconnected streets (connections to existing neighborhoods should be permitted only where practical and desired by those residents), sidewalks, etc., in an effort to encourage walkable neighborhoods within the Five Forks area.
2. Identify and re-utilize vacant buildings and properties that are no longer utilized:
- Encourage master planning of available land for redevelopment or new uses in order to promote shared parking, fewer entrances onto arterial roads, better utilization of land and increased open space.
 - Promote reuse and redevelopment of blighted and no longer utilized properties.
 - Target capital investments by James City County (e.g., infrastructure, underground utility lines, streetscape improvements, etc.) to support private reinvestment and redevelopment.
 - Through the Office of Housing and Community Development, investigate ways to renovate and rehabilitate the existing housing stock in the Five Forks area where appropriate. Work with private nonprofit groups such as Habitat for Humanity, the Community Action Agency and Housing Partnerships, Inc., to improve the condition and availability of the existing housing stock and assist residents that may be displaced by new development.
3. Reduce conflicts between incompatible land uses:
- Promote transitional uses between different land uses.
 - Through the rezoning/special use permit process and standards in the subdivision and zoning ordinance, reduce the impacts of higher intensity on lower intensity uses (requirements for landscaping, buffering, signage, screening, noise, odor, light, traffic, etc.).
4. Connect the land use pattern to a supportive, multi-modal transportation system:
- Establish compact, mixed-use development patterns that create a walkable environment and reduce the need to use the automobile by local residents.
 - Provide convenient pedestrian access from outlying residential areas to the Five Forks community activity center in accordance with Principle I.4.
5. Establish guidelines to define and maintain the historic, cultural, and aesthetic character of the Five Forks area:
- As part of the 2008 Comprehensive Plan update, designate Five Forks as a Community Character Area and incorporate the following guidelines as part of the Community Character element:
 - Building architecture, scale, materials, spacing, height, and color should respect the architectural context of existing structures such as the historic schoolhouse and veterinary

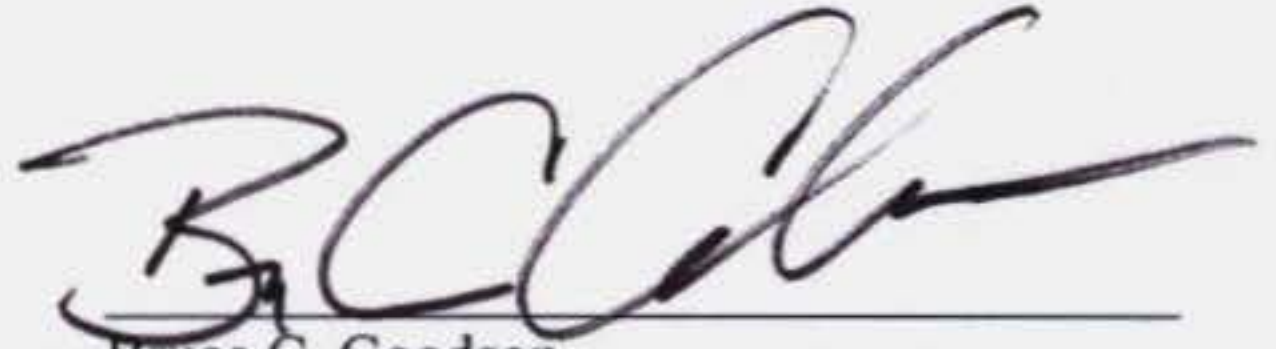
clinic and maintain the village character of Five Forks. New buildings should attempt to emulate distinguishing architectural elements of existing structures such as windows, roof lines, and cornices.

- Buildings that are traditional in character, massing, and detailing are preferred. Contemporary interpretations of traditional architecture are acceptable, if based on the scale and proportions of traditional architecture, and compatible with the context of the Five Forks village character.
 - Building facade materials and architectural treatment should be consistent on all sides of buildings, including side and rear elevations.
 - Where possible, parking should be located to the rear of buildings and should be well landscaped with shrubs and street trees. Shared access and parking should be pursued before constructing new access breaks and parking facilities.
 - Existing specimen trees and shrubs should be preserved to the extent possible. New landscaping should be of a type, size, and scale to complement and enhance the building and site design. Native plant and tree species are encouraged.
 - Signage should be of a scale, size, color, and materials to complement the village character of the area. Monument style signs, rather than pole signs, are the preferred type.
 - All mechanical equipment should be screened from view with architectural elements, fencing, or landscaping.
 - In addition to the above standards, residential buildings should have varied roof lines, wall articulations, window placements, and other features to reduce building mass and unbroken building lines. Arrangement and siting of buildings should preserve the buffers along the Community Character Corridor and complement existing structures such as the historic schoolhouse and maintain the village character of Five Forks.
- Develop and maintain defining traits that can be reflected through landscaping or streetscape design.
 - Protect and enhance the visual character of John Tyler Highway and Ironbound Road. Transportation improvements and new development should be carefully sited to minimize loss to the existing tree canopy over the roads.
6. Ensure that future residential and non residential development/redevelopment is compatible with the vision and principles for the Five Forks area:
- Ensure new trip generating developments do not exceed new trip thresholds in accordance with Principle I.5 through the rezoning/special use permit process.
 - Ensure proposed land uses are in compliance with the land use section of the 2003 Comprehensive Plan. The following descriptions provide additional guidance on acceptable land use proposals:

- Low Density Residential: Recommended gross densities are 1 to 3 dwelling units per acre. Higher densities should provide public benefits such as setting aside property for low-and moderate-cost housing developments; low- and moderate-income (Low income housing is defined as housing for persons earning less than 50 percent of area median income. Moderate income housing is defined as housing for persons earning 50 percent to 80 percent of the area median income.) housing; mixed-cost housing; or extraordinary environmental protection, including low impact design, better site design, open space preservation and implementation of the Powhatan Creek Watershed Management Plan.
 - Moderate Density Residential: Recommended gross densities are 4 to 10 dwelling units per acre. Higher densities should provide public benefits such as setting aside property for low- and moderate-cost housing developments; low-income housing (including persons earning less than 30 percent of area median income); moderate income housing; mixed cost housing; or extraordinary environmental protection, including low-impact design, better site design, open space preservation and implementation of the Powhatan Creek Watershed Management Plan. Recommended housing types include townhouses, apartments, or attached cluster housing.
 - Mixed Use: The recommended mix of uses includes offices and community commercial uses serving residents of the Five Forks area. Moderate-density housing may be a secondary use provided it is designed in accordance with these principles.
- As part of the 2008 Comprehensive Plan update, incorporate the above guidance into the Land-Use element.

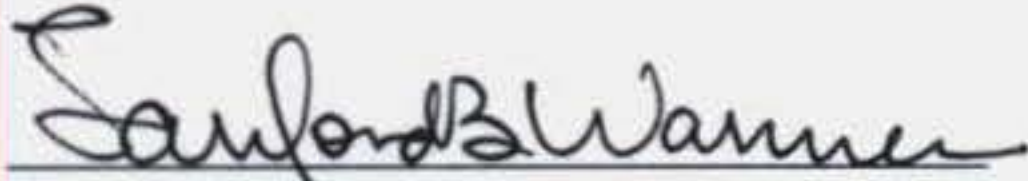
IV. Economic Development Principle

1. Promote and facilitate economic growth through development/redevelopment:
 - Facilitate the location of a new anchor tenant in Governor's Green Shopping Center should Winn-Dixie close.
 - Support the development of remaining undeveloped commercial land and vacant buildings in Five Forks to provide goods and services desired by residents of the Five Forks area.
 - Advise the Economic Development Authority on the outcomes of the Five Forks Study so that they may capitalize on future economic opportunities.



Bruce C. Goodson
Chairman, Board of Supervisors

ATTEST:



Sanford B. Wanner
Clerk to the Board

<u>SUPERVISOR</u>	<u>VOTE</u>
BRADSHAW	AYE
HARRISON	AYE
BROWN	NAY
MCGLENNON	AYE
GOODSON	AYE

Adopted by the Board of Supervisors of James City County, Virginia, this 28th day of September, 2004.

fiveforkredev.res